



# Harewood

## Neighbourhood Plan





## **City of Nanaimo**

### HAREWOOD NEIGHBOURHOOD PLAN

#### **Consolidated Version**

2013-OCT-28

All persons making use of this consolidation are advised that the amendments have been consolidated for convenience only, and that the original bylaws should be consulted for all purposes of interpretation and application of the bylaw.



## Acknowledgements

The Harewood Neighbourhood Plan was produced by the City of Nanaimo Community Planning Section with the assistance of the following:

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Special thanks to BC Hydro for their financial support of this neighbourhood plan.



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## Part A Introduction

- 1 Neighbourhood Plan Context
- 2 Purpose of the Neighbourhood Plan
- 3 Neighbourhood Profile



## 1 Neighbourhood Plan Context

The City of Nanaimo “OFFICIAL COMMUNITY PLAN BYLAW 2008 NO. 6500” (planNanaimo) was adopted in September 2008. Goal Seven of the Official Community Plan (OCP) speaks to neighbourhood and area planning, and the development of neighbourhood plans, as critical to achieving the objectives of the OCP.

This Neighbourhood Plan forms part of the City’s Official Community Plan. In British Columbia, the legal authority for local governments to adopt an OCP is derived from the *Local Government Act*. This *Act* outlines required content for community plans, primarily focused on the location, type and form of various land uses. The *Act* also provides local governments with the ability to address policies related to social issues, economic activities and the natural environment.

The OCP acts as a guide for decision making on planning and land management issues by City Council. It provides direction related to elements such as the location and type of residential, commercial and industrial development, the availability of transportation choices and utility servicing, and also considers environmental, social and economic issues.

All future land use decisions must be consistent with the OCP. However, the OCP is not a static document and the City may, from time to time, amend the OCP to adapt to new trends in the community or respond to changing conditions.

The OCP established the overarching city wide principle of striving toward a more sustainable Nanaimo and set key goals as follows:

- 1 Manage Urban Growth** by focusing urban development within a defined Urban Containment Boundary (UCB).
- 2 Build a More Sustainable Community** by creating urban nodes and corridors that support higher densities and a wider range of amenities and services than found in the surrounding residential neighbourhoods.
- 3 Encourage Social Enrichment** whereby Nanaimo is considered a socially sustainable community that nurtures a caring, healthy, inclusive and safe environment, and which empowers all of its citizens to realize their aspirations.
- 4 Promote a Thriving Economy** through efforts to grow and diversify the local economy, affording opportunities for residents and businesses, while staying within the capacity of the natural environment.

- 5 **Protect and Enhance Our Environment** by looking after Nanaimo's natural diversity of terrestrial, freshwater and marine ecosystems in the course of land use and development.
- 6 **Improve Mobility and Servicing Efficiency** by creating greater accessibility and more opportunity for safe and convenient movement around the city by transit, cycle and on foot.
- 7 **Work Towards a Sustainable Nanaimo** which is the critical goal for moving from "planning to action." Nanaimo strives to meet the vision of the OCP to build upon the strengths of the City and work to improve those areas where changes in economic, social and environmental conditions would create a stronger, more effective, sustainable city.



## 2 Purpose of the Neighbourhood Plan

The Neighbourhood Plan is intended to apply the broader goals and objectives of the OCP, including those related to sustainability, land use, social, economic, environmental, and mobility and servicing. Its purpose is to achieve the goals and objectives of the OCP while providing detailed policies to support the OCP within the planning area. It is intended to create a more livable neighbourhood.

The Neighbourhood Plan ultimately offers a long range vision for the neighbourhood that guides future development and improves quality of life for both neighbourhood and city residents as a whole. It addresses key issues for the neighbourhood area including mobility, neighbourhood character, land use, scale and form of development, density and view corridors, integration of land uses, and built form transitions and edges.

### Neighbourhood Plan Elements

- Specific land use designations within the neighbourhood
- Identification of environmental and other unique features
- Location of local services
- Local parks and open space
- Specific residential densities
- Road classifications and bicycle, pedestrian, sidewalk, trail, and other facilities
- Neighbourhood character and design guidelines

### 3 Neighbourhood Profile



#### Neighbourhood Plan Area

The Harewood neighbourhood is comprised of four distinct areas including:

- the Third Street area;
- the Central Harewood area;
- the South Harewood area; and
- the Harewood Mines Road area.

Covering a land area of approximately 535 hectares (1322 acres), the Neighbourhood Plan area is bordered to the south by the Nanaimo Parkway, to the east by the E&N Rail Line, to the north by the Western neighbourhood area, and to the west by Vancouver Island University, the Nanaimo Parkway, and community recreation centres. The area consists of a series of hills and valleys, with several watercourses running through the area. The geographical extent of the Neighbourhood Plan area is shown in Figure 1.

#### Land Use

The existing land use pattern consists primarily of single family and multiple family residential uses, with commercial uses occurring in pockets of the neighbourhood, particularly along Fifth Street, Wakesiah Avenue, Bruce Avenue and Fourth Street. Commercial uses are generally service oriented in nature. Some light industrial activity occurs on sites in the southeast corner of the neighbourhood plan area.

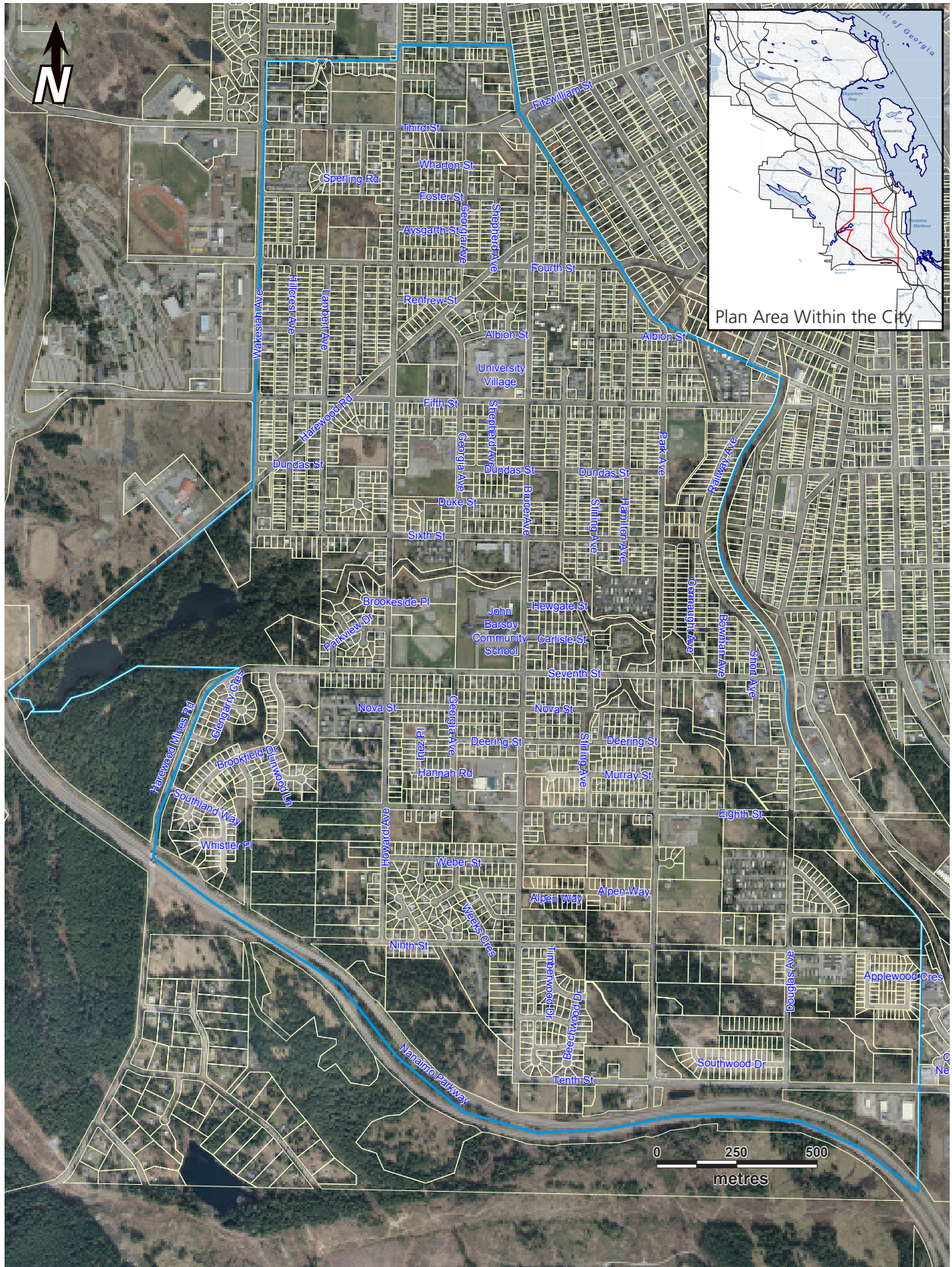


Figure 1 - Neighbourhood Plan Area

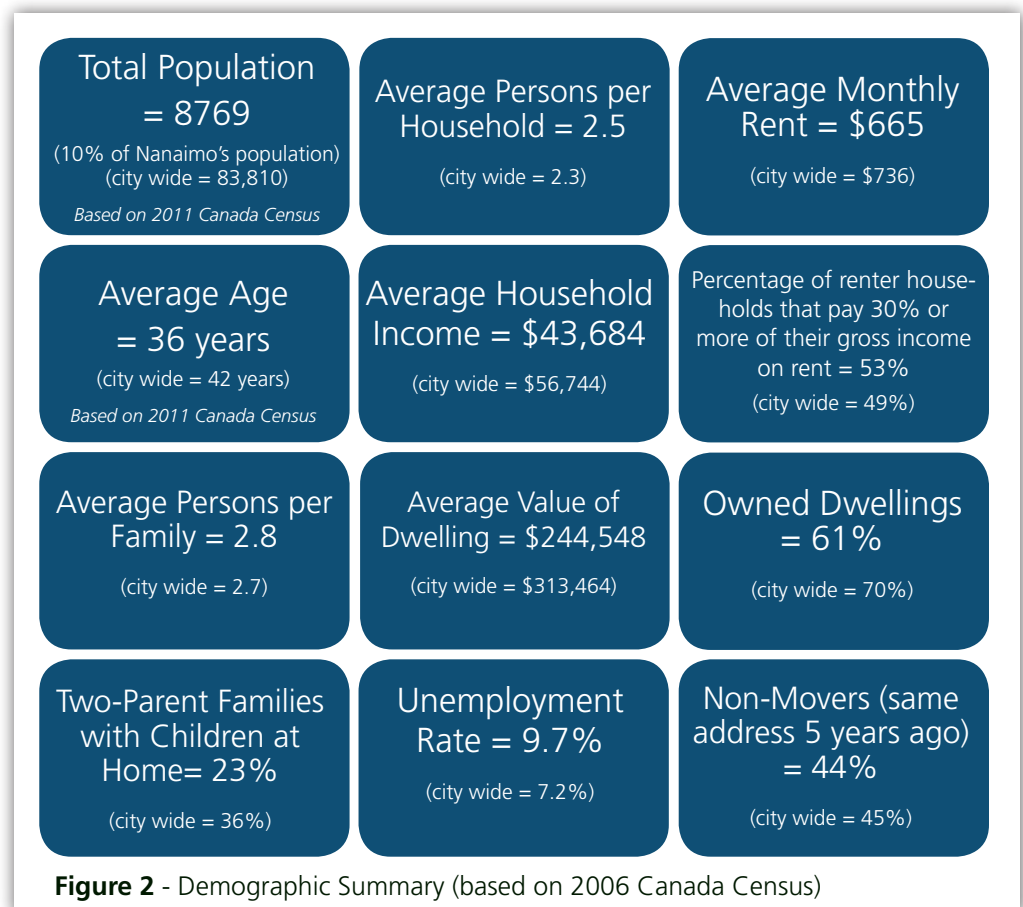


### Demographic Summary

The Harewood Plan area contains a population of approximately 8769 persons, as per the 2011 Canada Census data, representing 10 percent of Nanaimo’s total population. The area experienced a modest growth rate of 1.09 percent for the period of 2006 to 2011.

Population density within the neighbourhood is comparable to other areas within Nanaimo. At a total size of 535 hectares (5.35 square kilometres), the Harewood area has a residential density of 16 persons per hectare, as compared to nine persons per hectare for the City. At 2.5 persons per household, this represents approximately six residential units per hectare for the neighbourhood.

Other key indicators identified for the neighbourhood are as noted in Figure 2.





## Neighbourhood History

Harewood has a long and colourful history, dating back to pre-European settlement when the Snuneymuxw called the area Wakesiah (translated as 'far away'), and the valley was known as a place of peace and retreat. It acquired its current name in 1863 when Captain Horace Douglas Lascelles, an officer in the Royal Navy and related to the third Earl of Harewood, purchased 9000 acres of Crown land in the area with his partner Dr. Alfred Robson Benson. They subsequently formed the Harewood Coal Company after coal deposits were discovered in the nearby Harewood Ridges, southwest of the present day Harewood neighbourhood. After the death of Lascelles in 1869, the Harewood Coal Company found itself unable to compete with the Vancouver Coal Company or the Wellington Collieries, and the Harewood mine closed in 1877.

While there were no mines located directly within the neighbourhood plan area, an aerial tramway was introduced into Harewood in 1876 to support the nearby coal workings by carrying coal approximately three miles from Harewood to Cameron Island, for loading onto ships. The tramway's use was short lived however, and shut down when the Harewood Mine closed.

Settlement in Harewood began in 1886 with Samuel Robins, Superintendent of the Vancouver Coal and Mining Company. Considered a progressive mine company manager and the 'Father of Harewood', he maintained good relations with the company's miners, and subdivided Harewood into five acre parcels for their purchase or lease. Enough land was provided on which a miner's family could have a home and garden, a few cows, and security for the future. This was the origin of *Five Acres*, another name historically used for the area. Many of the parcels were leased or purchased and used as farms, with some of the more well known operated by the Calverley, Spencer, Thomas, Townsend and Spence families. One of the largest was the Dickinson Dairy Farm.

There were also a number of key milestones in the development of Harewood, leading toward its present day form:

- The first home in Harewood was constructed in 1892, on Howard Avenue near the Cemetery, by Richard Gibson.
- The first store was constructed in 1910, at the corner of Bruce Avenue and Fourth Street, by Mr. Storey and operated until 1913 when they were forced out of business by the great mine strike of 1912 to 1914.
- The first church, St. Paul's Harewood Mission, was built at the corner of Harewood Road and Howard Avenue.
- The first school was built in Harewood in 1892, and later replaced in 1914 by the Harewood School on Howard Avenue.





- Approximately half of Nanaimo's largest Chinatown was located in Harewood, along Pine Street at the edge of the neighbourhood's northeast boundary.
- A volunteer fire department was formed in 1942, with Henry Calverley as the first fire chief. An old BC Telephone truck was purchased and converted to a fire engine, and a firehall was constructed with materials obtained through demolition of the original North Harewood Elementary School.

Harewood continued to grow from these humble beginnings, and by the late 1940s had become the Harewood Improvement District. The suburban development of the neighbourhood continued throughout the 1950s and 1960s, and included construction of the Harewood Estates Plaza in 1958-1959 (now known as the University Village Shopping Centre). Harewood was subsequently amalgamated with the City of Nanaimo in 1975.

Over time, the neighbourhood has transitioned from rural farmland to a suburban residential neighbourhood whose land use pattern contains a mix of residential and commercial uses, and the remnant rural lands that remain evident today.

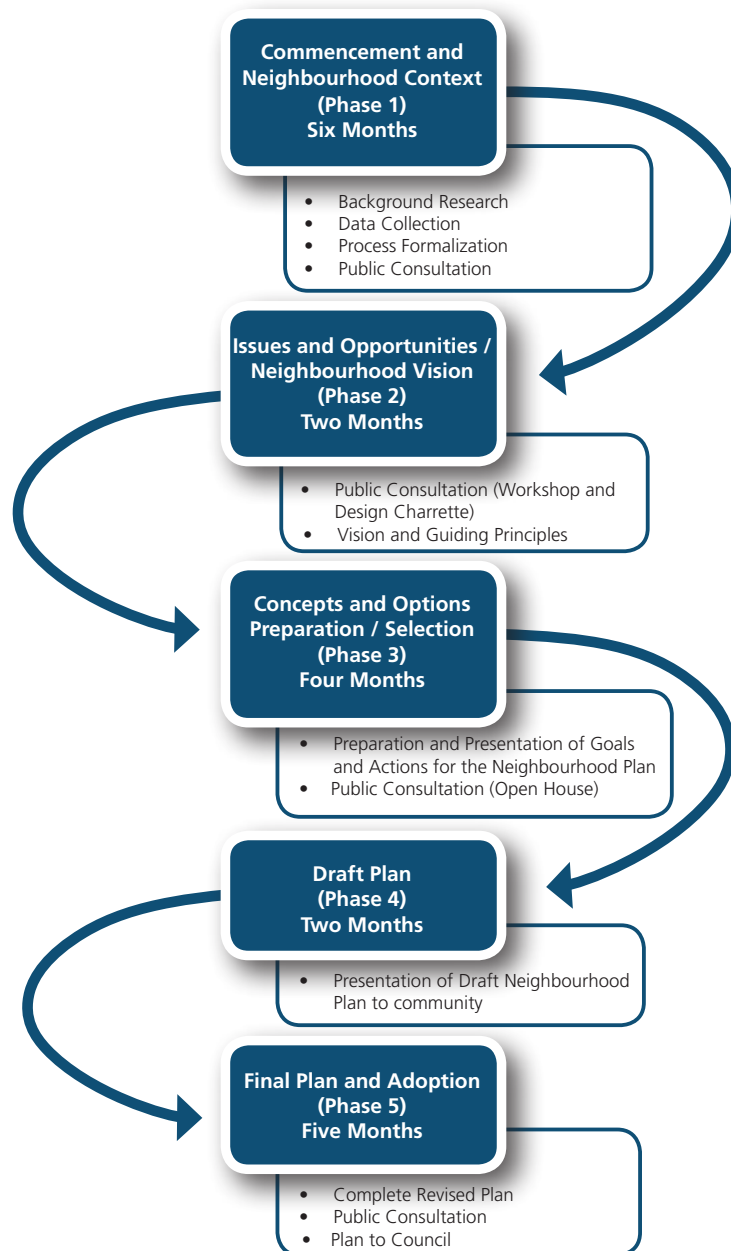
Development patterns since amalgamation have been characterized by continued small scale residential and commercial infill, and the subdivision of larger land parcels into single family residential lots, predominately in southern Harewood. Notable changes within the neighbourhood plan area have included realignment of Third Street in the 1990s, and redevelopment of the University Village Shopping Centre and construction of the first phase of the Tillicum Lelum Society Tenth Street Project in the late 2000s. Significant public investment also occurred in surrounding areas, including the Nanaimo Aquatic Centre, Nanaimo Ice Centre, and continued growth of Vancouver Island University.



## **Part B** Neighbourhood Plan Process

# 1 Neighbourhood Plan Process

On 2008-SEP-08, the Council for the City of Nanaimo adopted a new Official Community Plan (planNanaimo) that included a vision and guiding principles for the city in its entirety. One of the action items listed within this Official Community Plan (OCP) included the development of a Third Street Corridor plan, including Harewood. This neighbourhood plan process was initiated in 2012 and consisted of five distinct phases.



### Phase One: Commencement and Neighbourhood Context

Phase One of the process included initial background research and data collection, physical survey of the neighbourhood, and development of a community profile. Information included items such as demographics, existing land use and proposed development projects, topography and infrastructure.

The Harewood Neighbourhood Plan Committee was also initiated, including the following members:

- Neighbourhood Association / Neighbourhood Residents
- Education Community
- Local Senior and Youth Communities
- Local Business Community

To assist with development of the urban design framework and design guidelines for the Neighbourhood Plan, a team of design professionals was contracted and consisted of representatives from Urban Forum Associates and Ramsay Worden Architects.

Additional consultants were also retained to assist with other facets of the Neighbourhood Plan. Stantec Consulting was contracted to review and provide energy policy for the Neighbourhood Plan, with the financial assistance of BC Hydro; and Bunt & Associates to review, analyze and provide recommendations on transportation policy for the area.

Two public events were held to introduce the neighbourhood plan process. The first event was held 2012-MAY-26 at University Village Shopping Centre to promote awareness of the upcoming process. Phase One then concluded with a second Open House held on 2012-SEP-22 to provide more information on the planning process to the neighbourhood, of which approximately 180 people attended. Comments Sheets were made available to the public to receive their input.

### Phase Two: Issues and Opportunities / Neighbourhood Vision

Phase Two of the planning process focussed on substantial work with the Neighbourhood Plan Committee, including a series of meetings. This was followed by an Issues and Opportunities Workshop, held 2012-NOV-17, which provided an opportunity for the community to vocalize their concerns for the neighbourhood. Approximately 50 people participated in this facilitated Workshop, with a focus on open discussion as well as completion of worksheets. A group of Vancouver Island University students volunteered their time to help facilitate the workshop.





### **Phase Three: Concepts and Options Preparation / Selection**

Phase Three incorporated two opportunities for public input into the planning process. The first consisted of a two day design workshop held on 2012-NOV-30 and 2012-DEC-01. Facilitated by City staff and the consultants, approximately 33 participants from the neighbourhood identified issues and developed initial urban design concepts for the neighbourhood.

Along with continued discussion with the Neighbourhood Plan Committee, a Concepts and Options Open House was held on 2013-APR-13 to present work done to date, including draft guiding principles, land use options and plan concepts. As with the other public events, this Open House was well attended by over 180 people, with Comments Sheets made available to receive public input.

Again, a series of meetings was held with the Neighbourhood Plan Committee to discuss the progress and content of the neighbourhood plan.

### **Phase Four: Draft Plan**

City staff continued to meet with the Neighbourhood Plan Committee to further refine Neighbourhood Plan policies and land use options for the neighbourhood. The results of this work were incorporated into the Draft Neighbourhood Plan and presented to the community at an Open House held 2013-MAY-11. Approximately 120 people attended this Open House.

### **Phase Five: Final Plan and Adoption**

Following public consultation initiated in Phases One through Four, a final Open House was held 2013-JUN-08 to present the final Draft Neighbourhood Plan to the community. Approximately 65 people attended this Open House.

Phase Five also provided for consideration of the draft Neighbourhood Plan by the Nanaimo Advisory Planning Committee on 2013-JUN-18. The Neighbourhood Plan was subsequently adopted by Council as an amendment to the Official Community Plan.



## Part C Neighbourhood Sustainability

## 1 Neighbourhood Sustainability



A sustainable community can be described as “one that offers homes that are located near shops, schools, recreation, work and other daily destinations. These communities offer safe and convenient opportunities to walk, cycle or take public transit. This helps to reduce the time and money spent on driving, reduces greenhouse gas emissions, and makes efficient use of land and services. Inherent in this approach is increased housing density to generate a population base that will support commercial services, other utilities and the provision of public transit. In addition, this approach requires support for mixed use development so that a variety of uses can be found within a convenient distance and an investment in alternative modes of transportation including pedestrian and cycling trails which connect one place to another.” (planNanaimo)

At a base level, sustainability is considered to be the integration of environmental, social and economic interests. These interests can be considered on a range of levels from a global scale down to national, municipal and neighbourhood levels. For example, where greenhouse gas emissions are a global issue, policy addressing this issue can be found at provincial and municipal levels.

The City’s Official Community Plan in its entirety is based on the overarching principle of sustainability.

However, it is at the neighbourhood level that true forms of sustainable development and sustainable living can actually be realized; and taking action on achieving this can come from a number of sources, both private and public.

Residents of the Harewood Neighbourhood Plan area envision their neighbourhood as a sustainable one through initiatives to:

- ‘Green’ the neighbourhood
- Improve the habitat and water quality of local watercourses, wetlands and environmentally sensitive areas
- Promote stormwater remediation
- Provide for an active lifestyle through continued expansion of active open spaces and alternative transportation modes, particularly walking, cycling and transit
- Encourage local food production through urban agriculture techniques and education opportunities
- Offer compact, mixed use development that encourages a village character within the neighbourhood

These actions will continue to improve the area as a healthy, welcoming, self-sustaining community; and to meet the needs of future generations.



At the neighbourhood level, the concept of sustainability can be addressed through a range of actions, which are generally noted below.

- Reducing neighbourhood and individual household carbon footprint.
- Enhancing habitat biodiversity and managing water resources
- Recognizing and preserving both the natural habitat and cultural heritage features of the area.
- Ensuring a pedestrian scaled neighbourhood focusing on walkability and connectivity through:
  - providing short blocks and numerous pathways that effectively link the neighbourhood;
  - encouraging a mix of businesses and services that will provide a neighbourhood focus;
  - establishing a cost effective and convenient transit system;
  - offering both walking and bicycle routes, helping to reduce transportation costs and greenhouse gas emissions, thereby promoting a healthier community; and
  - ensuring safe and pleasant sidewalks and intersection crossings.
- Making the most efficient use of our streets, parking, and vehicle transportation structure by:
  - promoting low emission and small vehicles;
  - supporting car sharing opportunities;
  - encouraging carpooling and other trip reduction measures; and
  - enhancing opportunities for taxi, handyDART, and transit.
- Increasing residential density and providing a range of housing choices, helping to:
  - generate a population base to support local businesses, services and infrastructure;
  - achieve social sustainability through the creation of a more complete community;
  - house local residents of all ages, income levels, family structures and gender;
  - support local services and transit;
  - reduce greenhouse gas emissions; and
  - reduce service and infrastructure costs.
- Promoting green building design that includes adaptive forms of development, uses sustainable building materials, and ensures a compact, mixed use community structure.
- Reuse and adaptation of existing buildings to reduce waste in the landfill.
- Promoting energy sharing and district energy systems as important strategies for reducing greenhouse gas emissions.



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## Part D Guiding Principles

## 1 Guiding Principles

The Neighbourhood Plan is intended to address a series of topic areas identified by the community. These topic areas are highlighted through a series of 12 guiding principles, which were developed based on a series of workshops, open houses, and other input received from the community during the neighbourhood planning process. They form the foundation of the Neighbourhood Plan by reflecting those areas of importance identified by the area residents.



1 **Work Toward a Sustainable Community.**

An older, well established neighbourhood, Harewood residents are committed to building a vibrant community that balances social, economic and environmental factors. Sustainability will be achieved through best practices in transportation choices, building design and energy conservation techniques, densification, green infrastructure and open space.



2 **Develop a Strong Sense of Community.**

The Harewood neighbourhood is one of strong community ties, and is committed to preserving and enhancing its distinct sense of place. Development and activities are encouraged that improve neighbourhood vitality, promote a sense of community pride and of feeling at home, embrace multiculturalism, and encourage public enjoyment of local amenities and events.



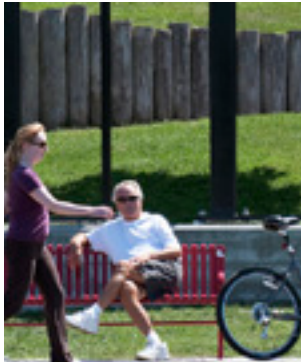
3 **Preserve Community Character.**

Harewood will strive to build upon its unique sense of place as one of Nanaimo's oldest neighbourhoods, with its distinctive topography, proximity to downtown and nearby public institutions, and its historically rural and agricultural character, and contribution toward the community's coal mining history. Character should be reflected through quality building design, scale of built form, and community improvements.



4 **Foster a Complete, Mixed Use Community.**

Harewood is a vibrant neighbourhood that encourages a mix of uses for living, working, shopping and playing within the area. It strives for self reliance while providing support and amenities to its residents, the University, and the greater community. Mixed use developments are particularly encouraged within the Corridor area of the neighbourhood.



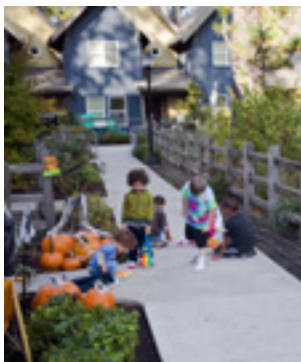
5 **Improve Connectivity.** With a well established road network and high volume of pedestrian and cyclist activity, Harewood will continue to expand its transportation choices through reinforcement and expansion of its grid pattern, sidewalks and laneways, dedicated network of pedestrian and cyclist routes, and increased use of local transit that connects throughout the neighbourhood and other parts of the community.



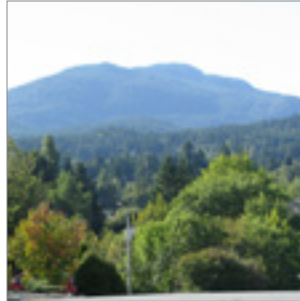
6 **Ensure a Mix of Affordable Housing.** Harewood is home to residents of differing ages, incomes and ethnic backgrounds. A mix of attractive, affordable housing choices must be provided throughout the neighbourhood that meets the needs of all residents, and provides opportunities for students, families, and aging in place.



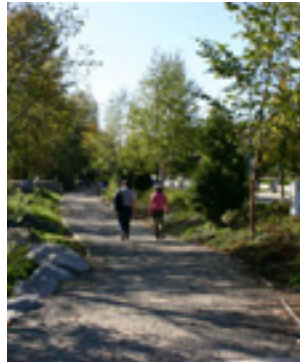
7 **Encourage Local Economic Activity.** A vibrant economy that supports and enhances the neighbourhood is highly encouraged. Local business and employment opportunities should contribute to products and services satisfying the needs of the neighbourhood, while encouraging innovation and adaptability. Commercial activity and community services are encouraged to locate within Corridor areas and local service centres to better serve the community.



8 **Promote a Healthy Lifestyle.** A healthy lifestyle is an integral part of a successful community. The Harewood neighbourhood supports the continued provision of quality parks and recreational opportunities, and the development of a sustainable food system that promotes food self sufficiency through education, engagement and land use activities.



9 **Protect Views and Vistas.** The Harewood neighbourhood strongly supports maintaining key views found within the area, and particularly those views to Mount Benson and the waterfront.



10 **Preserve and Enhance the Natural Environment and Open Spaces.** Harewood is a unique neighbourhood with watercourses, open spaces, hillsides and valleys. The neighbourhood is committed to improving its open space and environmental quality through expansion and connectivity of its parks and trails network, enhanced biodiversity and ecological improvements, and development of a green street program that enhances the ecological function of the streetscape.



11 **Provide Quality Meeting Places.** The Harewood neighbourhood supports additional meeting places throughout the neighbourhood that encourage community gathering and enrich the lives of local residents and visitors. Meeting places are identified as both outdoor and indoor venues that can be used for community meetings and activities, or as public places found along the streets for informal neighbourhood gatherings.



12 **Ensure a Safe Community.** The Harewood neighbourhood places great importance on crime prevention, public safety, and decreasing undesirable activities. Intensified efforts to create a safer community through neighbourhood involvement, crime prevention, maintenance and cleanup activities, and enhanced public spaces, are encouraged.



## Part E Neighbourhood Plan Policies

- 1 Introduction
- 2 Land Use and Development
- 3 Social Enrichment and Culture
- 4 Economic Development
- 5 Environmental Protection and Enhancement
- 6 Energy and Climate
- 7 Parks, Open Space and Connectivity
- 8 Transportation and Infrastructure

## 1 Neighbourhood Plan Policies Introduction



The following policy sections form the foundation of the Harewood Neighbourhood Plan, addressing specific topic areas in support of achieving the Guiding Principles set out in this Neighbourhood Plan.

The neighbourhood envisioned by these Guiding Principles, and supported through policy, will be a sustainable one with a strong sense of community. It is a vibrant, mixed use neighbourhood with a strong village character, mixed use developments, and a variety of residential building types. The neighbourhood will be supported through enhancement of the area's parks and open spaces, trails and greenways, biking and walking amenities, gathering places and public views.

An Urban Design Framework and Urban Design Guidelines further support Neighbourhood Plan policy by providing a means to address the area's built form and public realm.

Corresponding policy actions set out in the Neighbourhood Plan's Implementation Strategy will also help realize these guiding principles and policies. The vision for the neighbourhood will ultimately be accomplished through collaboration between the City and area residents, property owners and other stakeholder groups.

Policies are grouped together under the following key sections:

- Land Use and Development
- Social Enrichment and Culture
- Economic Development
- Environmental Protection and Enhancement
- Energy and Climate
- Parks, Open Space and Connectivity
- Transportation and Infrastructure

Maps 1 and 2 further support the policies of the Harewood Neighbourhood Plan.

*Where applicable, the implementation of policy actions will work with the relevant regulatory context such as OCP, zoning, BC Building Code, Manual of Engineering Standards and Specifications, and any other relevant standards and regulations.*



## 2 Land Use and Development

Policies contained within this section address the Neighbourhood Plan area's land use and built form. These policies are intended to enhance the Harewood neighbourhood as a unique place and better define its structure. Residential density is increased generally in the neighbourhood with a primary focus along the Corridors and along Fourth Street. Mixed use development is also directed to the Corridor and Commercial Centre areas. These commercial areas are supported by the increase in residential density. Two local service centres are also identified, providing for limited small scale services.

Fifth Street is identified as the new Harewood Village, the primary commercial focal point for the neighbourhood. Mixed use development with a commercial focus at the street level encourages a more vibrant pedestrian oriented area to promote a strong sense of community.



These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Preserve Community Character**
- **Foster a Complete, Mixed Use Community**
- **Ensure a Mix of Affordable Housing**
- **Encourage Local Economic Activity**
- **Promote a Healthy Lifestyle**
- **Protect Views and Vistas**

### Policies

#### Neighbourhood Land Use

- 1 Neighbourhood Land Use designations are presented on Map 1 – Neighbourhood Land Use, which forms part of the Neighbourhood Plan and is consistent with the objectives and policies of the Official Community Plan.
- 2 A mix of housing types in the neighbourhood will contribute to an overall healthier community. Housing in the form of single family homes, duplexes, triplexes, townhomes and apartments, as well as secondary suites, carriage houses, live / work studios, and seniors facilities, all serve to diversify the housing stock and provide housing for individuals and families of all age ranges and income levels.



- 3 Encourage the design of housing and neighbourhoods to be attractive, safe and livable.
- 4 Encourage laneway housing throughout the neighbourhood to increase residential options, improve affordability, and increase laneway animation.
- 5 New single family home construction and upgrades to existing single family homes are encouraged to respect the neighbourhood character by limiting the total floor area of a residence. Larger single family homes that are inconsistent with neighbourhood character are not supported.
- 6 Ensure infill projects throughout the neighbourhood are designed to complement the neighbourhood character.
- 7 Stand alone surface parking lots are not encouraged. Underground parking and / or under building parking is highly encouraged.
- 8 Enhancements to, and the creation of, additional parks, open spaces, and trails within all land use designations of this Neighbourhood Plan is supported.
- 9 Community facilities such as schools, churches, community centres, and social service agencies are supported in all areas of the neighbourhood.
- 10 To help satisfy the diverse and changing housing needs of residents, encourage the design of multiple family developments and individual units to be adaptive and accessible for people at different stages of life and mobility requirements.
- 11 Developments occurring on slopes should respect the natural topography of the site, responding to all environmental and natural constraints. Cut and fill excavations should be minimized by building with the slope.
- 12 Ecological features such as steep slopes, watercourses, riparian areas, and significant trees on environmentally sensitive lands are recognized as attributes of the site, and development shall proceed only where the impacts on these features are minimized and can be mitigated. New developments are encouraged to respect the characteristics of the site, including stepping down with the topography to minimize impact on views and alteration of the land. Where necessary, a system of smaller stepped retaining walls should be used instead of one large wall.

## Neighbourhood

- 13 Development in the Neighbourhood designation will be characterized by a mix of residential building forms including single family homes, multiple family units, coach houses and secondary suites, and special needs housing at residential densities of 10 – 50 units per hectare in 2 to 4 storey buildings.
- 14 This designation also supports the location of schools, community buildings, and local service centres within the neighbourhood.
- 15 Affordable housing should be provided in a range of forms including, but not limited to, seniors facilities, student housing, market housing, and subsidized housing.
- 16 Cluster housing forms are supported where the preservation of additional open space is provided, and which may encourage local food growing and production.

### *Local Service Centres*

- 17 A local service centre is supported in the general area of Bruce Avenue and Fourth Street. This centre will be characterized by small scale commercial and community services for neighbouring residents. Residential uses are also supported within the local service centre at residential densities of 10 - 50 units per hectare in 2 to 4 storey buildings.
- 18 A local service centre is supported in the general area of Tenth Street and Park Avenue. This centre will be characterized primarily by community services with limited commercial activity. Residential uses are also supported within this local service centre at residential densities of 10 - 50 units per hectare in 2 to 4 storey buildings.

## Townhome Neighbourhood

- 19 Development in the Townhome Neighbourhood designation will be characterized by a mix of residential building forms including multiple family units, seniors housing, student housing and special needs housing. Notwithstanding Neighbourhood policies contained within the OCP, residential densities within the Townhome Neighbourhood designation are supported at residential densities of 40 - 100 units per hectare in 2 to 4 storey buildings.
- 20 Development in the Townhome Neighbourhood designation will address the interface between the Townhome Neighbourhood and the adjacent Neighbourhood and Commercial Centres – Neighbourhood designations. Design elements of building siting, height and massing will be used to ensure a transition from the Townhome Neighbourhood to the adjacent Neighbourhood and Commercial Centres – Neighbourhood designations.





### **Residential Corridor**

- 21 The Residential Corridor designation is located primarily along Bruce Avenue. Development in the Residential Corridor is characterized by a mix of residential building forms, at residential densities of 50 – 150 units per hectare in 2 to 4 storey buildings.
- 22 Development in the Residential Corridor designation will address the interface between the Residential Corridor and Neighbourhood designations. Design elements of building siting, height and massing will be used to ensure a transition from the Residential Corridor to the adjacent Neighbourhood designation.

### **Mixed Use Corridor**

- 23 Development in the Mixed Use Corridor will be characterized by a mix of uses including multiple family residential development, public amenities, commercial, professional and service uses in mixed use developments, with residential densities of 50 – 150 units per hectare in 2 to 6 storey buildings.
- 24 Stand alone multiple family residential developments are supported within the Mixed Use Corridor at residential densities of 50 – 150 units per hectare in 2 to 6 storey buildings.
- 25 Commercial services within mixed use developments shall be supported in the Mixed Use Corridor designation. In mixed use developments, ground floor uses will be retail, office or community uses that invite public activity. Residential uses will be provided in upper storeys; professional uses in upper storeys are also supported.
- 26 Stand alone commercial developments and drive-thru facilities are not supported in the Mixed Use Corridor, and zoning to permit these forms of development is not supported.
- 27 Commercial services within mixed use developments are supported along Wakesiah Avenue, and particularly between Fourth Street and Fifth Street. Additional commercial services within mixed use developments are also supported in the area between Third Street and Fourth Street.
- 28 Commercial services within mixed use developments are generally supported along Third Street, and particularly at the Third Street and Howard Avenue intersection.
- 29 Development in the Mixed Use Corridor designation will address the interface between the Mixed Use Corridor and Neighbourhood designations. Design elements of building siting, height and massing will be used to ensure a transition from the Mixed Use Corridor to the adjacent Neighbourhood designation.

### Village Corridor

- 30 The Village Corridor will promote the Harewood Village along Fifth Street, with development characterized by a mix of uses that will promote a village character, including multiple unit residential development, public amenities, and commercial and service activity in mixed use developments, with residential densities of 50 - 150 units per hectare in 2 to 6 storey buildings. Commercial uses and other related professional or service activity are required on the ground floor, with residential above.
- 31 The Village Corridor will also promote a village character for an area along Bruce Avenue and Eighth Street, adjacent to the Commercial Centres – Neighbourhood designation. Development will be characterized by a mix of uses that will promote a village character, including multiple unit residential development, public amenities, and commercial and service activity in mixed use developments, with residential densities of 50 - 150 units per hectare in 2 to 6 storey buildings. Commercial uses and other related professional or service activity are required on the ground floor, with residential above.
- 32 Development in the Village Corridor designation will address the interface between the Village Corridor and Neighbourhood designations. Design elements of building siting, height and massing will be used to ensure a transition from the Village Corridor to the adjacent Neighbourhood designation.
- 33 Stand alone commercial developments and drive-thru facilities are not supported in the Village Corridor, and zoning to permit these forms of development is not supported.



### Commercial Centre - Neighbourhood

- 34 Development in the Commercial Centre - Neighbourhood areas will be characterized by a mix of uses with a primary focus on commercial activity. Uses supported in this designation include commercial, professional and service uses, public amenities, and multiple family residential development at residential densities of 50 - 150 units per hectare.
- 35 Building forms of 2 to 6 storeys are permitted. Stand alone commercial buildings up to 2 storeys are also supported.
- 36 Development in the Commercial Centre - Neighbourhood designation will address the interface between the Commercial Centre - Neighbourhood and adjacent residential areas. Design elements of building siting, height and massing will be used to ensure a transition from the Commercial Centre - Neighbourhood to the adjacent residential areas.



### **Parks and Open Space**

- 37 The City's Parks, Recreation and Culture Master Plan is the guiding document for the provision of parks and open space in the city, and will be updated to reflect the policies set out in this Neighbourhood Plan.
- 38 Existing Parks and Open Space lands, as well as possible future park land are identified on Map 1 – Neighbourhood Land Use. The locations of potential future park land are intended to indicate general location. Their identification is intended to assist the City and land owners to anticipate long term plans, and for the City to negotiate the purchase or dedication of park land.
- 39 Park acquisition is prioritized as the following:
  - Existing field at Fifth Street and Howard Avenue.
  - Wetland area at Park Avenue and Eighth Street for natural open space.
  - Land area generally north of Lambert Avenue at Fourth Street for an active recreation area.
  - Land areas adjacent to, and that complement, adjacent park areas and watercourses.

### **Light Industrial**

- 40 Development in the Light Industrial area will be characterized by a mix of uses including, but not limited to, manufacturing, warehousing and distribution, equipment sales and repairs, construction and transportation. Uses that create noise or emissions, operate at hours incompatible with residential uses, require outdoor storage space or are unsightly, or use, generate or store hazardous materials, are not supported.
- 41 A substantial buffer of either natural or man made features, or uses that provide adequate transition to residential or other uses, will be provided on industrial sites.
- 42 Industries adjacent to residential sites should be compatible in nature, and should consider factors such as siting, height and screening.
- 43 An increase to the industrial land supply is not supported beyond those lands already designated Light Industrial or permitted by zoning.

### 3 Social Enrichment and Culture

Policies contained in this section encourage enrichment of the neighbourhood's social and cultural development. The neighbourhood's artistic and cultural resources are of particular importance to the community, as are specific policies around the provision of affordable housing and community programs for residents. Community spaces should be enhanced, with new ones created; and public art should be installed, both to enhance the neighbourhood and to provide a sense of welcome for residents and visitors alike.

Social service and safety issues are addressed, with policy focusing on providing affordable market and non-market housing; and working with local organizations and other stakeholders to reduce criminal activity, encourage property improvements, and develop additional educational and recreational programs for local residents. Communications amongst the neighbourhood association and other organizations such as Vancouver Island University and the School District will also be promoted.

These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Develop a Strong Sense of Community**
- **Preserve Community Character**
- **Ensure a Mix of Affordable Housing**
- **Promote a Healthy Lifestyle**
- **Provide Quality Meeting Places**
- **Ensure a Safe Community**

#### Policies

##### Community Gathering Places and Public Art

- 1 Encourage the provision of indoor and outdoor public meeting spaces, enhancing opportunities for residents to gather in a safe and attractive environment. These meeting spaces should provide opportunity for community gathering, art displays, films and live performances, celebrations of local heritage and community identity, and educational / recreational programs.
- 2 Encourage the development of additional community centre(s) within the neighbourhood, either through new construction or redevelopment of existing sites. Preference is given to renovation of the Harewood Activity Centre, and utilization of the Harewood School site.



**Potential Heritage Properties**

15 Albion Street  
 350 Bruce Avenue  
 370 Bruce Avenue  
 425 Bruce Avenue  
 625 Bruce Avenue  
 631 Bruce Avenue  
 684 Bruce Avenue  
 796 Bruce Avenue  
 588 Bruce Avenue  
 807 Douglas Street  
 305 Elizabeth Street  
 307 Elizabeth Street  
 105 Fifth Street  
 488 Fifth Street  
 585 Fifth Street  
 188 Fourth Street  
 302 Fourth Street  
 418 Fourth Street  
 435 Fourth Street  
 529 Fourth Street  
 530 Fourth Street  
 604 Fourth Street  
 435 Hamilton Avenue  
 660 Hamilton Avenue  
 297 Harewood Road  
 324 Howard Avenue  
 349 Howard Avenue  
 350 Howard Avenue  
 365 Howard Avenue  
 495 Howard Avenue  
 824 Howard Avenue  
 840 Howard Avenue  
 496 Ninth Street  
 615 Park Avenue  
 271 Pine Street  
 489 Seventh Street  
 499 Seventh Street  
 394 Shepherd Avenue  
 530 Sixth Street  
 660 Third Street  
 692 Winchester Avenue  
 694 Winchester Avenue

**Potential Heritage Landscape Features**

601 Bruce Avenue - English oak  
 442 Fourth Street - Maple

- 3 Support the development of a local library within the neighbourhood area.
- 4 Consider the policies of the Nanaimo Cultural Strategy and the Community Plan for Public Art (2010) in implementing a public art program for the Harewood neighbourhood. Specific items to address include:
  - Encourage artistic activities in strategic locations to provide for a positive street presence in a livable neighbourhood.
  - Consider the incorporation of public space that provides opportunity for performing arts and displaying public art. The provision of a community stage is encouraged.
  - Incorporate public art into new developments and streetscapes, particularly along the Corridors, Fourth Street, and at Gateways.
  - Consider the implementation of a signage program unique to Harewood, with signage appropriately placed at Gateways and other key locations within the neighbourhood.
  - Consider the installation of a community information board in an area that receives high visibility.

**Heritage Character**

- 5 The review and placement of additional neighbourhood historical buildings and sites to the Community Heritage Register, as listed in Figure 3, will be supported. Properties of historical interest that are not on the heritage register are encouraged to develop in a manner that is mindful of the area’s historic character.
- 6 The review and placement of additional neighbourhood heritage landscape features to the Tree Protection Bylaw, as listed in Figure 3, will be supported.
- 7 Sensitive infill on a heritage site is supported where the heritage of that site, including buildings, structures and landscape, is protected. Where historically significant features such as buildings and landscape are present, incorporate these features into new development, as appropriate.
- 8 Encourage the reuse of the neighbourhood’s historic sites and buildings, and particularly where it is feasible to reuse buildings for multiple family residential units, community use, commercial use or professional office space.

**Figure 3** - Potential Heritage Properties



### Social Services / Affordable Housing

- 9 Support the development of criteria for the setting of social and health services within the neighbourhood.
- 10 Ensure affordable housing, seniors housing, residential care homes, student housing and other special needs housing is available to all age groups and all income levels. Affordable housing should be provided throughout the neighbourhood.
- 11 The construction of affordable housing units within new and existing residential developments is encouraged.
- 12 Disperse non market housing so it is not concentrated in any one particular area.
- 13 As part of an overall affordable housing strategy, the City will work to set a specific affordable housing unit target per new development.

### Food Systems

- 14 The City will support local groups in the creation and operation of local community gardens and food security initiatives, thereby enhancing urban agriculture opportunities in the community. Small scale food production may occur in various forms, including but not limited to community gardens, urban orchards, and backyard gardening.
- 15 Encourage cluster housing to accommodate larger open spaces for food production. Additional densities may be supported where lands are set aside for food opportunities.
- 16 Encourage edible landscapes in new commercial and multiple family residential developments, as well as in single family, institutional, and parks settings.
- 17 Consider the development of a 'street gardening program' on local neighbourhood streets, for the use of local residents to plant boulevards with vegetables, fruits, flowers, and other native pollinators.
- 18 Encourage the planting of fruit and nut trees in City parks and open spaces, as appropriate. Opportunities to utilize boulevard areas will also be explored.
- 19 The City will explore additional methods for supporting urban agriculture within the community.
- 20 Consider a review of the City's zoning bylaw to allow for additional food production opportunities where they appropriately fit into the neighbourhood.
- 21 The siting of a local farmers market in the Neighbourhood Plan area is supported.





### Safety and Security

- 22 New development or redevelopment of existing properties should consider CPTED (Crime Prevention Through Environmental Design) principles to provide for enhanced personal safety.
- 23 Neighbourhood stability and continued security will be provided through the combined efforts of the City's Bylaw Services Department, RCMP, and neighbourhood residents. The City will support local neighbourhood groups and the RCMP in:
  - Providing regular police patrols of the neighbourhood
  - Developing local neighbourhood watch programs
  - Reducing criminal activity within the area, including property damage, drug use and prostitution
- 24 The City will support the neighbourhood to resolve issues with unsightly buildings and nuisance properties.
- 25 The City will support local neighbourhood residents and businesses in establishing 'adopt a block' or neighbourhood cleanup programs.
- 26 Increased safety and reduced light pollution will be pursued through installation of energy efficient street lighting.

### Seniors and Youth

- 27 The City will work with the neighbourhood association to develop park, recreation and culture opportunities in the neighbourhood that include both a senior and youth focus, and promote universal accessibility in the design of park, trail and roadway improvements.
- 28 The City will work with the neighbourhood association and neighbourhood residents to review and determine which, if any, support services are necessary for seniors and other special needs residents living in the neighbourhood.
- 29 Ensure boulevard treatments provide amenities suitable to the neighbourhood residents, which may include park benches and wheelchair accessible sidewalks.
- 30 The City will work with the neighbourhood to create a child friendly urban environment.
- 31 The City will work with the neighbourhood association and neighbourhood residents to review and, where appropriate, increase the number and variety of recreational and educational programs for young children and teens.
- 32 The neighbourhood association and school district will continue to work collaboratively to develop the community schools program, meeting the needs of the school district and the local neighbourhood.

## Communication

- 33 The City will periodically meet with the neighbourhood association to evaluate the progress of the Neighbourhood Plan, determine if any changes are needed, and to discuss any issues of concern to the neighbourhood.
- 34 The City will continue to consult with the neighbourhood association where development proposals have been submitted for the area.
- 35 The neighbourhood association will work with the City to promote community events, festivals and other activities in the area.
- 36 The neighbourhood association will work with the City and neighbourhood residents to establish a broader base of the association's membership, work toward and maintain registration under the *Society Act*, and build stability and organizational capacity.
- 37 Promote increased communication between the City, neighbourhood, School District 68, Vancouver Island University, and the Ministry of Transportation and Infrastructure, for the purposes of information sharing and partnership on servicing, security, community building, planning and arts and culture initiatives.



## 4 Economic Development

Policies contained within this section focus primarily on support for promoting economic health and sustainability within the neighbourhood. Commercial areas sited within the Neighbourhood Plan area service not only neighbourhood residents, but also adjacent institutions due to their accessibility.

Policies support economic activity within the Neighbourhood Plan area, encouraging additional employment opportunities for local residents, promoting Harewood as a vibrant, welcoming and unique neighbourhood, and encouraging additional uses to locate within the neighbourhood. Development incentives to support these uses will also be explored.

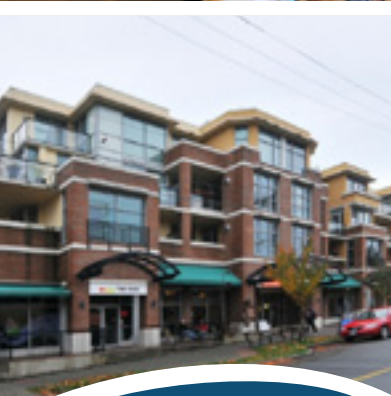
These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Develop a Strong Sense of Community**
- **Encourage Local Economic Activity**
- **Preserve and Enhance the Natural Environment and Open Spaces**

### Policies

#### Community Economic Development

- 1 Explore the feasibility of establishing a Development Incentive Program designed to encourage commercial and residential investment within the Neighbourhood Plan *Harewood Village* (Fifth Street corridor) area, and to encourage local businesses to upgrade facades and attract business.
- 2 Increased economic activity and employment, particularly within the Mixed Use Corridor, Commercial Corridor, Commercial Centres – Neighbourhood, and local service centres, will be encouraged. Fifth Street will be promoted as the village commercial area within Harewood, encouraging a mix of small scale pedestrian oriented uses such as retail stores, restaurants and services.
- 3 Encourage mixed use developments along Wakesiah Avenue, between Fourth Street and Fifth Street, that provide commercial and professional service amenities to service local residents and the adjacent University population.



- 4 Support the development of two local service centres within the Neighbourhood designation, each with distinct characteristics, and located at:
  - Bruce Avenue at Fourth Street
  - Tenth Street at Park Avenue
- 5 Encourage a variety of small, independent businesses, professional services, and home based businesses to locate within the neighbourhood, and that respond to the local needs of the community; and particularly to support the development of medical offices, financial institutions, and a local library.
- 6 Support the development of employment training programs to help alleviate unemployment and poverty issues within the neighbourhood.
- 7 The City will pursue appropriate public amenities and infrastructure upgrades for the neighbourhood where developments are proposed, as noted in the Development Amenities section of this Neighbourhood Plan.
- 8 Build upon a relationship with Vancouver Island University to continue to develop programs and offer services within the neighbourhood.



### Neighbourhood Image

- 9 Support a marketing strategy to rebrand Harewood as a vibrant, welcoming and unique neighbourhood.
- 10 Encourage the development of an informational brochure that promotes Harewood as a great place both to live and to invest.

## 5 Environmental Protection and Enhancement



Policies contained within this section focus on the improvement of environmental quality, and enhancing the area's natural resources. Of particular importance to the neighbourhood is the improved health of the area's watercourses, and 'greening' of the neighbourhood. Policies address this through best management practices, community engagement, street tree and native species plantings, and restoration activities. Ongoing educational opportunities and interaction with the community on environmental issues will be pursued.

These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Develop a Strong Sense of Community**
- **Preserve and Enhance the Natural Environment and Open Spaces**

### Policies

#### Watercourses and Water Quality

- 1 Pursue expansion of watercourse setbacks to provide additional protection for local watercourses, and provide public access to walking trails along watercourses. New developments along watercourses should provide public access to allocated areas along watercourses, where feasible, and undertake restoration of the watercourse as necessary. Construction of trails and provision of public access will meet Provincial and federal regulations.
- 2 Steep slopes and wetlands should be retained, where possible, to protect natural features and water recharge and habitat areas.
- 3 Encourage daylighting (de culverting) to restore and enhance watercourses, and to contribute to a rainwater strategy. Specific consideration should be given to daylighting the watercourse along Stirling Avenue.
- 4 Pursue alternative rainwater management techniques to mitigate rainwater runoff directly into neighbourhood watercourses. Where runoff systems are on private property, maintenance procedures should be developed and submitted to the City. In addition, opportunities to set up a monitoring program to assess the effectiveness of any installed system should be considered.
- 5 Provide interpretive signage to identify watercourses, and to explain rainwater management systems and their purpose in improving water quality and protecting fish habitat. Identify the importance of not releasing oils, grease and soap into storm drains due to their direct impact on local watercourses.

### Green Neighbourhoods

- 6 Pursue the placement of street trees throughout the neighbourhood, and particularly within the Corridor areas. Consider planting historic street tree species such as English Oak and Copper Beech.
- 7 Work with neighbourhood groups to set a target for tree plantings within the Neighbourhood Plan Area.
- 8 Encourage environmentally friendly drought tolerant landscape species, native and otherwise, that promote water conservation, and allow for solar gain through all seasons.
- 9 Undertake the removal of invasive vegetation in the Neighbourhood Plan Area and encourage replanting of appropriate species, and particularly native species where the location will support them.

### Environmental Awareness

- 10 The City will partner with local organizations in educational opportunities and environmental activities such as invasive species management, watercourse restoration, community gardens, and street tree planting, and that also promotes community involvement.
- 11 Encourage the expansion of recycling programs within the Neighbourhood Plan Area.

### Environmental Monitoring and Resource Conservation

- 12 Promote the installation and restoration of habitat, and particularly within environmentally sensitive areas located within the Neighbourhood Plan area; and pursue enhancement of the ecological function of local streetscapes, parks, and other public places. This can be accomplished through use of the City's online tools such as the Habitat Atlas.
- 13 The City will pursue funding under available infrastructure grant programs to further implement green street improvements within the Neighbourhood Plan area such as street trees and alternative rainwater systems.





## 6 Energy and Climate

Energy use and climate change are issues that communities will be facing into the future. Addressing these will assist the Harewood neighbourhood to be more resilient to energy price increases, and will globally help curtail greenhouse gas emissions. Through the OCP and the Community Sustainability Action Plan (CSAP), the City has made commitments to work with residents and businesses to conserve energy and reduce carbon emissions. Policies in these sections focus on finding ways to build more efficient buildings, implement renewable energy sources, and reduce vehicle related emissions. These actions are well aligned with policies contained throughout this Neighbourhood Plan.

These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Improve Connectivity**
- **Encourage Local Economic Activity**

### Policies

#### Land Use and Transportation

- 1 Encourage the use of sustainable design principles when reviewing applications for development. Utilize the Sustainable Design Guidelines to assist with the review.
- 2 Encourage preferential parking for car poolers and / or low emission vehicles in commercial areas.
- 3 Encourage the allocation of on and off street parking for small scale transportation including motorcycles, scooters, electric bicycles and very small cars (for example, 3 metre parking spots).
- 4 Encourage commuting reduction and parking reduction strategies in new developments.
- 5 Promote alternative transportation forms that encourage walking, cycling and transit mobility, and reduce greenhouse gas emissions and energy use.



### New Development and Existing Buildings

- 6 Encourage an integrated design process that works to identify opportunities to reduce energy use.
- 7 The City will support efforts by applicants to conduct design charrettes or other innovative input sessions, early in the development process or prior to a formal application, to explore energy reduction opportunities and alternative energy sources.
- 8 Encourage high energy efficiency and other green building performance features in new development.
- 9 Explore energy sharing opportunities within mixed use developments to maximize heat capture.
- 10 Promote the use of energy efficiency incentives during the design and planning stage of development.
- 11 Promote building designs that achieve cooling through passive design and natural ventilation in order to reduce electrical cooling requirements.
- 12 Encourage the use of energy efficient components, fixtures and appliances, including hot water systems and high efficiency laundry.
- 13 Discourage the use of electric baseboard heating, where possible.
- 14 Consider designs that are flexible to allow for the future installation of renewable energy technologies such as solar hot water heating, micro wind turbines, or solar photovoltaic cells.
- 15 Encourage the installation of suitable electric vehicle (EV) charging infrastructure within commercial developments.
- 16 Promote landscapes and exterior design that enhances solar gain in winter and reduces solar gain in summer.
- 17 Residents are encouraged to improve the efficiency of their own buildings by utilizing available energy efficiency programs such as offered through BC Hydro or other energy organizations.
- 18 Encourage waste reductions and material reuse through the reuse of existing buildings, or through the use of building materials from demolished buildings. Where heritage protection is important, heritage and revitalization tax exemptions can be used and incorporate energy efficiency.
- 19 Encourage the Central Vancouver Island Homebuilders' Association to continue their efforts in promoting the Built Green program.
- 20 Encourage builders to create 'future ready' homes at the construction stage by:
  - Incorporating the features of solar hot water ready systems
  - Providing suitable wiring to car parking areas to allow for future installation of Level 2 electric vehicle charging stations.





### **Alternative Energy and District Energy**

- 21 Support the application of on site electric photovoltaic (PV) solar panels. Review regulations to allow for PV and hot water solar installations without diminishing development options such as height or Floor Area Ratio (FAR).
- 22 Encourage the use of building scale renewable energy technologies (for example, solar hot water, geo exchange) in new developments.
- 23 The City will work with the Vancouver Island University (VIU) as they explore campus wide or broader district energy systems. The VIU Campus Master Plan identifies renewable district energy as a goal to be pursued on the campus and preliminary investigations have been made. At present, it is expected that future campus buildings will be developed to be compatible with a district energy system. Major developments along Wakesiah Avenue or Fifth Street might be able to connect to such a system in the future.

### **Street Lighting and Energy Standards**

- 24 The City will review street lighting levels throughout the Neighbourhood Plan Area in order to identify those areas subjected to inadequate lighting, and to identify opportunities to implement energy efficient street lighting technologies.
- 25 Where laneways exist or are introduced into the Neighbourhood Plan area, street lighting levels will be evaluated and updated with pedestrian scaled light poles equipped with Low Energy streetlight technology, where warranted and as budget allows.
- 26 The City will continue to work with BC Hydro and developers to evaluate where candidates for power line burial may exist under BC Hydro's power line burial program, and how these projects can be facilitated in cooperation with the City of Nanaimo and / or development proposals.

### **Emissions Reduction**

- 27 The City will work with the neighbourhood association and other stakeholders to establish a series of actions to meet the City's greenhouse gas emissions targets. These actions could include:
  - Greenhouse gas emissions baseline inventory
  - Specific emissions reduction plan
  - Neighbourhood level performance goals for solid waste, transportation, energy use, air emissions, water use, open space protection and creation, tree planting and building constructionall as part of the city wide Sustainability Action Plan and green design guidelines.

## 7 Parks, Open Space and Connectivity

A key element of the Neighbourhood Plan is the importance of parks, public open space and related amenities. Neighbourhood residents stress the importance of nature trails connecting through the area, as well as the provision of a range of parks.

Neighbourhood Plan policies focus on protecting and enhancing the neighbourhood's parks, and ensuring a connected network of trails and open spaces throughout the area. A variety of park types are encouraged, including nature parks, fitness parks, sports fields, and small neighbourhood parks. Other policies address the maintenance of key views focusing both within and outside the neighbourhood.

These policies are intended to be consistent with and build upon and inform the goals and objectives of other City parks and open space policy found in the Parks Master Plan, Trail Implementation Plan, Urban Forest Strategy, Community Plan for Public Art and Manual of Engineering Standards and Specifications.

These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Foster a Complete, Mixed Use Community**
- **Improve Connectivity**
- **Promote a Healthy Lifestyle**
- **Protect Views and Vistas**
- **Preserve and Enhance the Natural Environment and Open Spaces**
- **Provide Quality Meeting Places**



### Policies

#### Trails and Neighbourhood Connectivity

- 1 An efficient and extensive system of walkable and bike friendly routes along trails and roads throughout the neighbourhood will be pursued. These connectivity opportunities will be encouraged via signage, road markings and road access improvements between parks and open spaces, and along trails and existing streets as applicable.
- 2 The City will work with and support the Island Corridor Foundation in continued development of the E&N Trail as a vital link between the neighbourhood and the downtown area.



### **Parks and Open Space**

- 3 Encourage the development of parks, including natural parks, off leash dog parks, fitness parks, sports fields, and small neighbourhood parks within the community. Residents should be within 400 metres of a small neighbourhood park to ensure accessibility for local residents.
- 4 Continue to maintain and / or upgrade existing active recreation areas such as Robins Park and Harewood Centennial Park; and nature areas such as Colliery Dam Park. Encourage the redevelopment of Harewood Centennial Park as a multi generational campus and outdoor community gathering space with a youth focus and sport tournament facilities.
- 5 Increase the play and fitness equipment available in the area. Specifically consider addition of a basketball court, playground equipment, and general fitness equipment for residents of all ages.
- 6 Promote the siting of open spaces and urban plazas along the Corridor areas to encourage informal gatherings.
- 7 Pursue beautification projects through the City's Volunteer in Parks program, to encourage, for example, park improvements, implementation of community gardens, tree plantings, and adopt a tree initiatives.
- 8 Develop a parks and trails system that recognizes the unique and natural setting of the neighbourhood, and considers accessibility for all mobility levels.
- 9 Encourage the installation of trails along local watercourses, consistent with Provincial and federal regulations; and provide access to the watercourses, where appropriate.
- 10 Undertake the removal of invasive vegetation in the Neighbourhood Plan area parks and replace with park approved plant species, where appropriate.
- 11 Support the implementation of an interpretive signage program that identifies and educates the community on natural areas, local wildlife, and heritage sites.
- 12 The City will continue to work with School District 68, the Community Schools program, and Tillicum Lelum Society to pursue further use of open space and other facilities as multi use recreational space.

### **Views**

- 13 Maintenance of key panoramic and street end views, as noted in the Urban Design Framework section of this Neighbourhood Plan, will be considered in the design of new development.
- 14 Building forms that respect the topography of the area, and massing of adjacent building forms, will be encouraged through the development review process.

## 8 Transportation and Infrastructure

Streets throughout the neighbourhood should be safe and efficient for all modes of transportation, from pedestrian to cyclist and driver. A transportation network for the Harewood area should accommodate all residents of the area, providing safe and efficient access to employment, shopping, educational and recreational activities. Efforts to accomplish this should focus on methods of traffic calming, enhanced street character, and ensuring adequate routes for sustainable, alternative modes of transportation.

Also of importance to this neighbourhood is the maintenance and / or upgrade of the existing infrastructure. Policies respect the community's desire for a continued move toward a sustainable infrastructure, including natural rainwater management systems.



These policies support the following guiding principles identified in the Neighbourhood Plan process:

- **Work Toward a Sustainable Community**
- **Improve Connectivity**
- **Encourage Local Economic Activity**

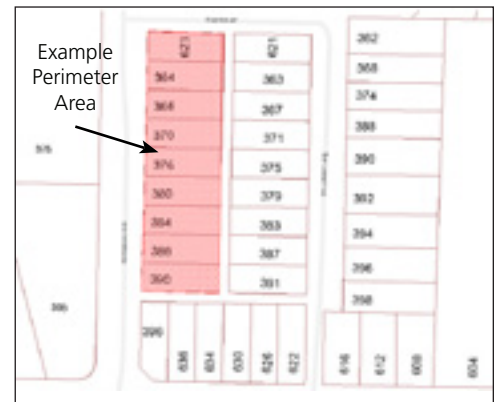
### Policies

#### Transportation System

- 1 The City's existing major Road Classifications for the Neighbourhood Plan area are maintained and enhanced as shown on Map 2 - Road Network.
- 2 The City's existing major Road Classifications for the Neighbourhood Plan area are defined within the Official Community Plan. A number of proposed changes to better reflect the current and future role and function of the Harewood street network are suggested by this Plan. The City will consider these changes through the Nanaimo Transportation Master Plan process. Proposed changes, as noted in Figure 12, include:
  - Bruce Avenue is currently classified as a Major Collector with a future four lane cross section. Consider keeping Bruce Avenue at its current two lane cross section, with left turn bays at intersections.
  - Upgrade Howard Avenue from Neighbourhood Collector to Minor Collector between Fifth Street and Seventh Street, to reflect the changes to the role of Bruce Avenue, and to support future development south of Seventh Street.
  - Develop a Neighbourhood Collector to connect Tenth Street and Bruce Avenue to Harewood Mines Road through future development.



- Reduce the classification of Ninth Street, between Park Avenue and Bruce Avenue, to Local Street.
  - Continue the Neighbourhood Collector classification of Douglas Avenue through to the east end of Eighth Street as future development permits.
- 3 Street infrastructure should complement the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) while seeking to incorporate the design principles laid out within the Neighbourhood Plan.
  - 4 Encourage a multi modal transportation network for the neighbourhood, as laid out in Figures 12 to 16 of the Urban Design Framework.
  - 5 Continuation of the area's historical street grid system will be pursued for the entire neighbourhood as development and redevelopment opportunities occur. Cul de sac and crescent street forms are not generally supported.
  - 6 A small block pattern with a target block perimeter of up to 600 metres, (as defined by public right of way and measured around the perimeter of a continuous group of lots), is encouraged for future developments to promote accessibility and walkability by maintaining small, permeable blocks that are easy to walk through or around.
  - 7 In general, road and infrastructure improvements will be completed as adjoining private land development occurs.
  - 8 On major roads, left turn access will not be supported except at intersections.



Block Perimeter Diagram

### Harewood Village

- 9 The future design of Fifth Street within the proposed *Harewood Village* should complement and support the envisioned land use. This area will include sidewalks, street trees, landscaped boulevards and medians, and other design features intended to reinforce the pedestrian orientation of this street and development of laneways either side of Fifth Street to provide alternative rear access to parcels.

### Laneways

- 10 The creation of laneways to provide access and servicing to adjacent developments, provide informal cycling and pedestrian connections, and to provide transition zones to adjacent areas is strongly encouraged throughout the neighbourhood; and particularly on either side of Fifth Street.
- 11 Encourage continued maintenance of laneways throughout the neighbourhood.
- 12 New laneway design should be consistent with CPTED principles.
- 13 Where laneway houses are constructed, require landscape features along the laneway.

### Gateways

- 14 Through a combination of public investment and private development, gateway signage, public art, and landscape will be considered for the entrances to the neighbourhood, and particularly along: Third Street at Wakesiah Avenue and Bruce Avenue; Fifth Street at Wakesiah Avenue and Railway Avenue; and Tenth Street, as shown on Figure 6.

### Pedestrian Greenways, Sidewalks and Street Character

- 15 Pedestrian greenways, as identified in Figure 8, are encouraged in order to promote greater pedestrian and cyclist activity. Priority will be placed on improving Georgia Avenue, Bruce Avenue, Fourth Street, Fifth Street and Wakesiah Avenue as development along these streets occurs.
- 16 Establish a pedestrian friendly streetscape along major streets by installing landscape, street trees, street furniture, and open spaces suitable for resting or socializing. Priority will be placed on improving those streets with the highest activity levels and pedestrians present.
- 17 An efficient and extensive system of walkable and bike friendly routes along trails and roads throughout the neighbourhood will be pursued. These connectivity opportunities will be encouraged via signage, road markings, bike amenities and road access improvements between parks and open spaces, and along existing streets as applicable.
- 18 The provision of continuous sidewalks, separated from the street by a landscaped boulevard, is supported and encouraged throughout the neighbourhood. Priority areas for new construction should be given to areas with high pedestrian activity, including commercial areas of Fifth Street, Bruce Avenue, Fourth Street, Wakesiah Avenue, and Eighth Street. Where sidewalks are not feasible, consider alternative pedestrian standards.





- 19 Encourage schools to make walking and cycling to school a better option for students.
- 20 Consider the accommodation of scooters and other mobility equipment when constructing new pedestrian infrastructure.
- 21 Encourage street sign standards that specifically highlight pedestrian and bicycle way finding routes.

### **Traffic and Pedestrian Safety**

- 22 Pedestrian routes are encouraged, as identified in Figure 8, in order to promote greater pedestrian and cyclist activity.
- 23 Consider traffic and pedestrian safety upgrades for the intersections at Bruce Avenue and Fifth Street, Fourth Street at Pine Street, and Third Street at Howard Avenue.
- 24 Encourage both on and off street cycling routes including dedicated on street cycle lanes along major roads.
- 25 Consider additional crosswalks with flashing signals along major roads with particular focus near school sites.

### **Transit**

- 26 Continue to work with the Regional District of Nanaimo and the BC Transit Authority to establish an efficient local transit system that provides enhanced service connecting Harewood to Vancouver Island University, Downtown and beyond, and as laid out in Figure 15.
- 27 Enhance bus stops with pull outs, pedestrian amenities and weather protection.

### **Rainwater Management**

- 28 On local streets where drainage ditches are present, establish a program to upgrade and enhance the drainage areas with efficient, natural rainwater swales as an alternative to culverts.

### **Water and Sanitary Sewer**

- 29 When redeveloping existing lots, sewer and water services, and their connections to the street, should be inspected to determine physical condition. Any necessary repairs should be completed as part of redevelopment of the site.

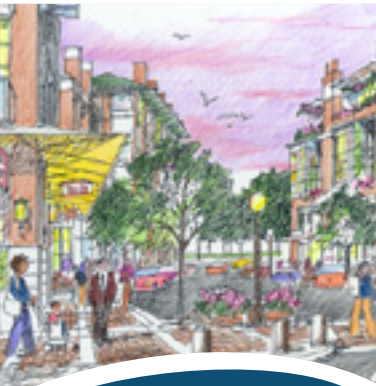




# Part F Urban Design Framework and Guidelines

- 1 Urban Design Framework
- 2 Urban Design Guidelines

## 1 Urban Design Framework



The Urban Design Framework and Design Guidelines are intended to provide a connection between the neighbourhood planning policies and development regulations. Offering guidance on how future development can be carried out in accordance with the neighbourhood plan policies, this section should be consulted by developers, architects, neighbourhood groups and planning staff when considering building form and streetscape design. The Urban Design Framework and Design Guidelines are supplemental to the Neighbourhood Plan policies.

The Urban Design Framework for the Harewood Neighbourhood Plan area is intended to provide overall direction on neighbourhood wide systems and elements such as pedestrian connectivity and public open space, view protection, watercourse protection, neighbourhood edges and gateways, land uses and building form, and transportation modes.

The Framework also identifies typologies of new development, and provides general direction on commercial, mixed use, and residential development.

The intent of the Urban Design Framework is to establish the broad urban design strategies for the Harewood Neighbourhood Plan area, so that other more site specific and building specific elements can then be integrated in a consistent manner. The Urban Design Framework establishes the context for the more specific Urban Design Guidelines that follow.

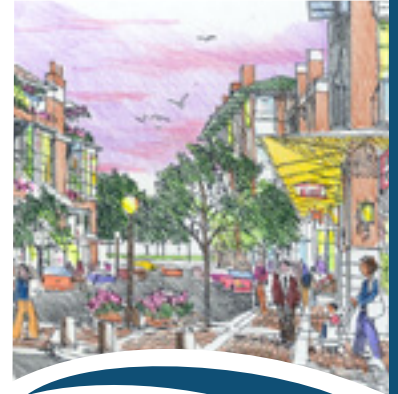
The Framework establishes a hierarchy of pedestrian oriented streets and trails throughout the neighbourhood, an expanded network of public open spaces that are connected by these routes and trails, a strategy for protecting and enhancing watercourses, and identifies the key public views (both street end and panoramic) that should be protected with future development. The Framework also identifies those areas of the neighbourhood that could support higher residential densities, and provides strategies for implementing a multi modal transportation network framed around the principles of sustainability.

Implementation of the Urban Design Framework will result in a more coherent, complete and sustainable neighbourhood that is focused on:

- Two mixed use Neighbourhood Commercial Centres (at Fifth Street and Bruce Avenue, and Eighth Street and Bruce Avenue, respectively)
- A mixed use precinct along Fifth Street – *Harewood Village* and other Corridor designated areas
- A broader range of residential housing typologies in Neighbourhood designated areas, including options for protecting more open space
- An enhanced public realm and proposed land reserves
- Improved access to the adjacent citywide multi use trails along the Nanaimo Parkway and the E & N Railway line.

It also suggests improved connections to neighbouring areas, opportunities for urban agriculture, and additional green space and trails along the Chase River and other existing creeks.

This Urban Design Framework should be read in conjunction with the Harewood Neighbourhood Plan Urban Design Guidelines, which it complements.



## 1.1 Corridor Development

The Harewood Neighbourhood includes a Corridor designation where mixed use development is encouraged to concentrate, including new multiple family residential development and public amenities.

Harewood's Corridor designated area is focused along the following major streets:

- Bruce Avenue
- Fifth Street
- Third Street
- Wakesiah Avenue

There are two existing Neighbourhood Commercial Centres or nodes within the Corridor area that have concentrations of commercial uses:

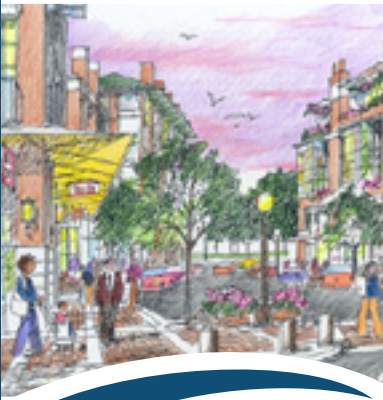
- University Village at Fifth Street and Bruce Avenue
- Eighth Street at Bruce Avenue

There is also another small commercial node located outside of the Corridor area, at Fourth Street and Bruce Avenue.

Each of these precincts or nodes is somewhat different from the others, and this Urban Design Framework acknowledges and builds on these distinctive characteristics, while also aiming to create a common high quality pedestrian environment along all Corridor designated streets.

### Key Strategies

- 1 Provide a high quality pedestrian environment along all Corridor streets, including consistent streetscape treatment. This includes well designed sidewalks, pedestrian lighting, landscape, street furniture, public art, signage, and other amenities.
- 2 Encourage a distinct identity for each Corridor designated street through built form and streetscape design, to contribute to neighbourhood character and diversity.
- 3 Encourage an appropriate transition in building uses, heights and massing between Corridor development and adjacent residential neighbourhoods.



**Corridor Development**

- OCP Corridor Designation Boundary
- Corridor
- Neighbourhood
- Light Industrial
- - - Mixed Use Precincts
- ★ Commercial Nodes
- ⊙ Community Uses
- Commercial at Grade
- Local Commercial / Services
- Ⓢ School
- ▨ Residential with Choice of Use on Ground Floor

**Third Street Precinct**

- Predominately residential character
- Choice of use with an increased front setback at grade.
- Located between gateways at Wakesiah Avenue and Bruce Avenue.
- Steps down in scale to relate to adjacent lower density residential neighbours.
- Benefits from its proximity to Cat Stream and proposed public open spaces at Howard and Wakesiah Avenues.

**Wakesiah Precinct**

- A mixed use street relating to the adjacent University with choice of use / commercial at grade.
- Steps down in scale to relate to lower density neighbourhood to the east.
- Bounded by gateways on Third Street and Fifth Street.
- Benefits from the University population and services and proximity to the Cat Stream at the north end of Wakesiah Avenue.

**Harewood Village Commercial Node**

- Envisioned as the heart of the Harewood Neighbourhood.
- Proposed mixed use street with commercial at grade and residential supporting local shops and services.
- Potential location for local community events and festivals.
- Builds on the existing commercial centre at Bruce Avenue and Fifth Street.
- Proposed community facilities and gathering places near Harewood Road.
- Benefits from proposed public green spaces including community gardens and a proposed range of land uses.

**Eighth Street Commercial Node**

- Builds on existing commercial development.
- Forms a neighbourhood gateway at the south end of the Corridor.
- Proposed adjacencies include residential developments with choice of use at grade and a potential community facility.
- Benefits from its proximity to nearby water ways and proposed expanded network of trails.

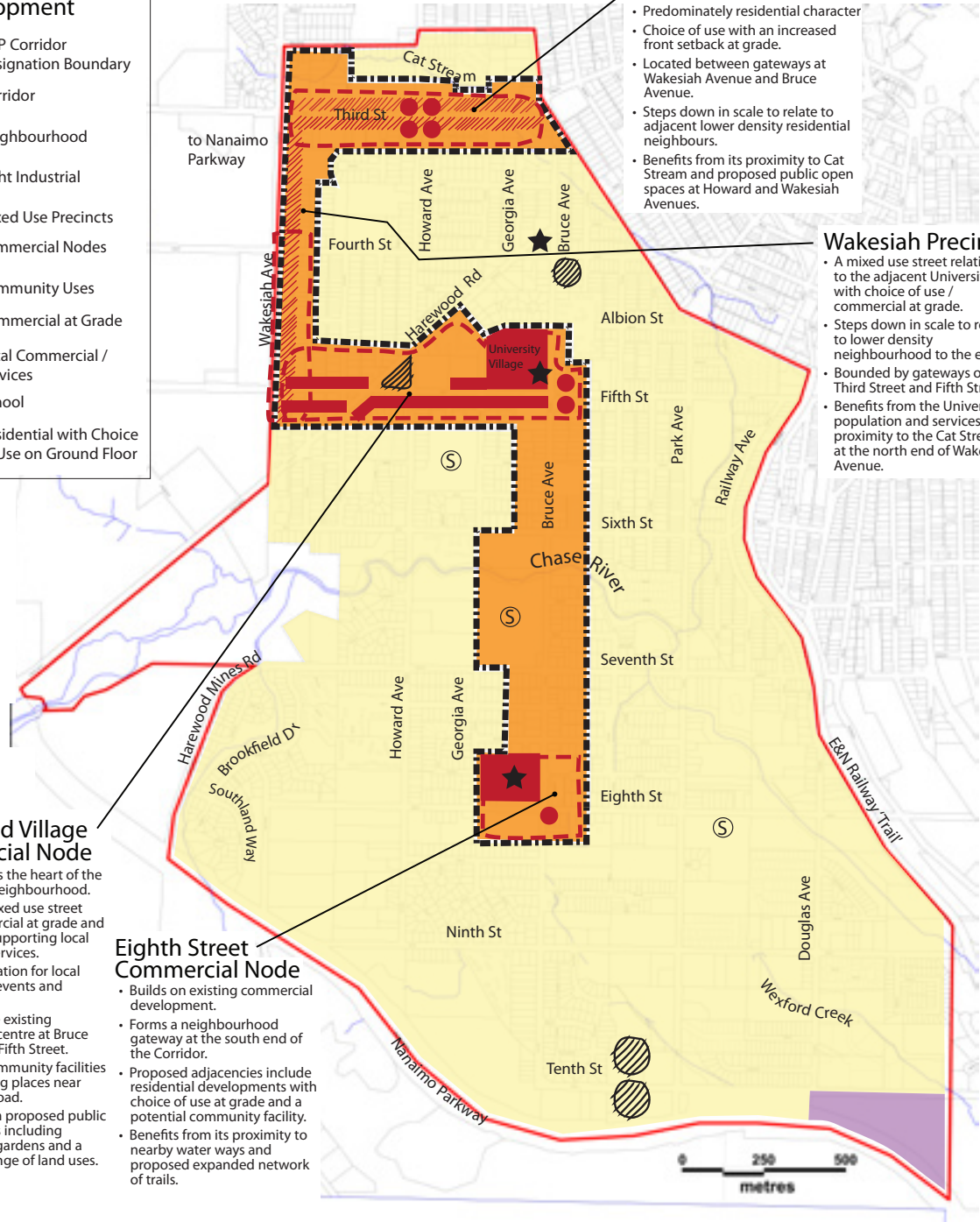
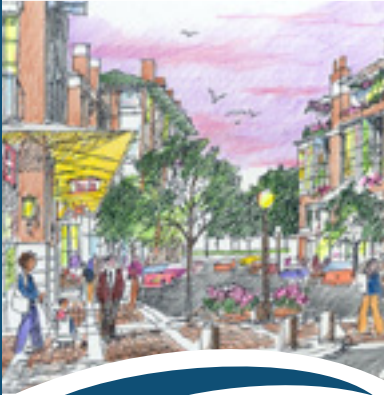


Figure 4 - Corridor Development



### Third Street

- 4 Encourage primarily residential development, with some local serving retail and services at key areas along this street.
- 5 Encourage forms of development that step back from the street to protect views of Mount Benson.

### Wakesiah Avenue

- 6 Encourage local and university serving retail and services to locate between Fourth Street and Fifth Street, opposite Vancouver Island University. Development north of Fourth Street to be 'choice of use' (includes Live / Work) at ground level.
- 7 Encourage building forms that provide for urban plazas along the street and between buildings, and allow for connectivity to adjacent laneways and neighbourhoods.
- 8 Consider building height and massing to protect public views of Mount Benson from the adjacent residential areas.

### Fifth Street

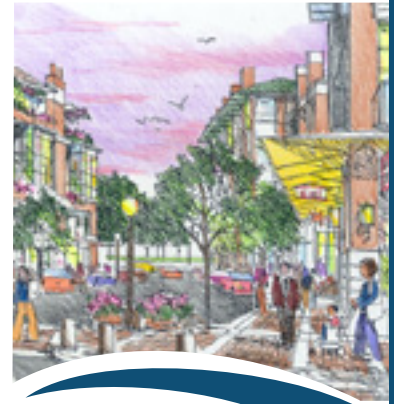
- 9 Encourage development of Fifth Street as the *Harewood Village* commercial core of the neighbourhood, with a range of uses including commercial retail and services, community uses, and multiple family residential development.
- 10 Build on and complement the existing Commercial Centre at Bruce Avenue and Fifth Street, by concentrating additional commercial uses around this intersection.
- 11 Focus community uses on the publicly owned triangle of land between Fifth Street, Harewood Road and Howard Avenue, and the existing park space at Fifth Street and Howard Avenue.
- 12 Encourage development of a mixed use commercial / residential Gateway at the intersection of Fifth Street and Wakesiah Avenue, as the major entry transition between the University and *Harewood Village*.
- 13 Encourage forms of development that step back from the street to protect views of Mount Benson.

### Bruce Avenue

- 14 Focus multiple family residential development along Bruce Avenue between the two existing Commercial Centres and John Barsby Community School.
- 15 Develop a high quality pedestrian streetscape environment along this major north south arterial corridor, with well designed sidewalks, lighting, street trees and other landscaping. Develop rainwater management strategies that minimize direct runoff into the Chase River.

### Fourth Street / Bruce Avenue Commercial Node

- 16 Encourage retention and enhancement of the small commercial node at Fourth Street and Bruce Avenue (the 'triangle' site). This node provides local serving goods and services and this Neighbourhood Plan supports this node being retained.
- 17 Support existing and new local serving commercial uses on the triangle of land bounded by Fourth Street, Harewood Road and Bruce Avenue; and near the Bruce Avenue and Fourth Street intersection.



## 1.2 Residential Development

A wide range of housing types and choices is supported by the Corridor and Neighbourhood designations in the OCP. Providing increased housing choice while maintaining neighbourhood character is key to creating a more complete and sustainable neighbourhood that supports aging in place, a range of socio economic levels, and diverse household formations.

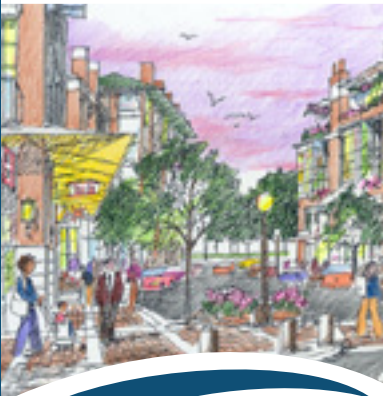
### Key Strategies

#### Neighbourhood Designated Areas

- 1 Maintain the overall lower scale, lower density character of the Harewood neighbourhood, with infill housing forms such as narrow lot housing, coach housing, laneway housing and duplex housing, that is compatible with the existing housing character; and provided it meets the intent and densities outlined in the OCP.
- 2 Ground oriented multiple dwelling housing forms may also be provided that complement the scale and character of existing single family homes in the neighbourhood designation. This includes townhousing, row housing, and cluster housing.
- 3 Encourage cluster housing forms, including semi detached and attached housing, that aim to protect larger parcels of undeveloped land, high value ecosystems such as riparian zones and wetlands, or lands that could be retained for urban agriculture and community gardens. Such developments shall be permitted the same density rights as traditional development forms that use the entire development parcel, with potential for density bonusing, but the permitted density shall be concentrated on a smaller portion of the subject development parcel, in order to facilitate land preservation.
- 4 Encourage housing forms, designs and materials that respect and celebrate the rural / agricultural traditions of Harewood.
- 5 Consider permitting some higher density attached housing forms that maintain high level views at the edge of the neighbourhood plan area, following the topographic ridge along the southwest edge of the area.
- 6 Support higher density residential development along Fourth Street, with townhouse and row house typologies, with this street forming a transition to Vancouver Island University and Downtown Nanaimo.

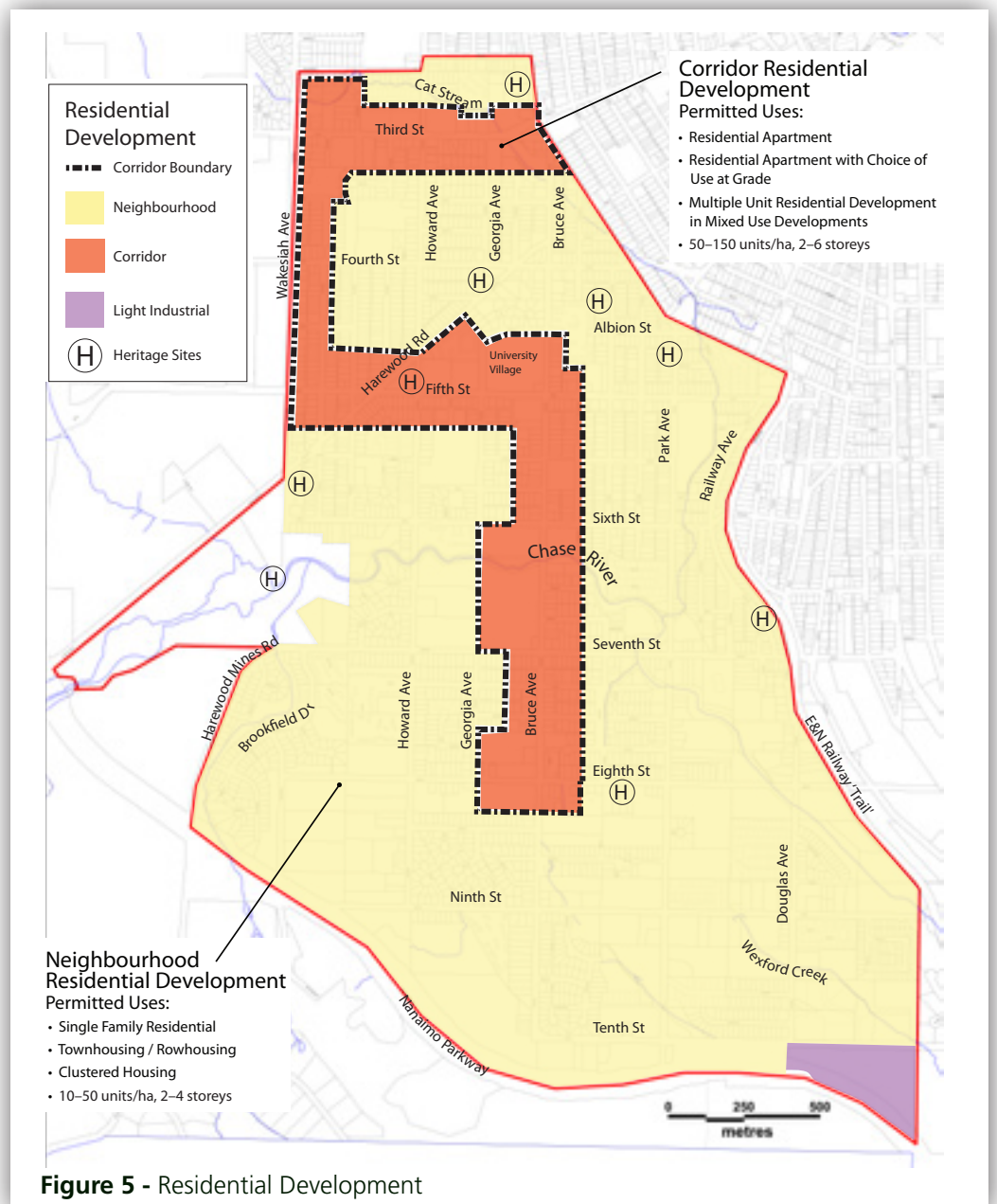
#### Corridor Designated Area

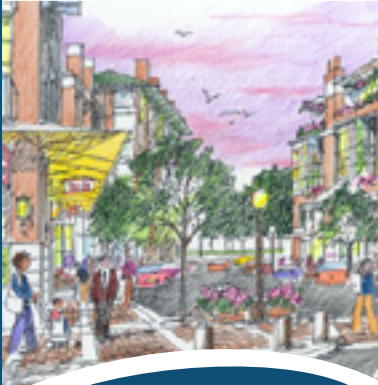
- 7 Concentrate higher density multiple dwelling housing along the Corridor designated streets.
- 8 Permit higher density residential within the Commercial Centre - Neighbourhood designation, above ground level.
- 9 Permit up to 6 storeys in special circumstances where such building height does not detract from street end views and a very high design standard is achieved.





- 10 Permit residential with 'choice of use' (commercial or residential) on the ground floor fronting on the Corridor designated streets, as shown on Figure 4.
- 11 Require commercial uses at grade along Corridor designated streets, as shown on Figure 4.
- 12 Permit, but do not require, commercial uses at grade along Corridor designated streets, as shown on Figure 4. The ground and second floors of these areas shall permit a 'choice of use' including Live / Work uses (home based businesses), or purely residential uses.
- 13 Encourage housing forms that step back from the key east west Corridor streets (Third Street and Fifth Street) to optimize protection of Mount Benson views.





### 1.3 Gateways and Edges

Harewood is well defined by visual and physical boundaries, which in many areas are quite significant. The neighbourhood is also defined by city wide linear movement corridors on two sides, the E & N Railway line and the Nanaimo Parkway. These, together with the Colliery Dam Park area, result in a limited number of entry points to the neighbourhood from the surrounding parts of Nanaimo.

These conditions combine to provide opportunities for Gateway elements that would help define the Harewood neighbourhood and announce arrival, and to enhance connections to adjacent areas and the linear movement systems.

Maintaining, strengthening, and increasing connections to adjacent neighbourhoods and other parts of the city is important for future development of the neighborhood. It also enhances livability for Harewood residents.

#### Key Strategies

- 1 Create Gateway features at key entry points to Harewood, at the following locations and as noted on Figure 6:
  - Third Street and Wakesiah Avenue
  - Fifth Street and Wakesiah Avenue
  - Bruce Avenue, Third Street and Fitzwilliam Street
  - Fifth Street and the E & N Railway line
  - Fourth Street and Albert Street
  - Seventh Street and Douglas Avenue
  - Tenth Street (at east end)
  - Harewood Mines Road (at Nanaimo Parkway)
- 2 Gateway features could include signage, public artwork, and other elements that serve to announce arrival in Harewood and relate to its history and character.
- 3 Installation of gateway features may require acquisition of additional lands, where appropriate.
- 4 Develop new and / or enhance existing pedestrian connections to the future E & N Railway trail to the east, and to the Nanaimo Parkway trail to the south.
- 5 Develop new and / or enhance existing pedestrian connections to the Colliery Dam Park area to the west.

- Gateways and Edges**
-  Gateway
  -  Park Edge and Enhanced Connections
  -  Nanaimo Parkway Trail and Enhanced Connections
  -  E&N Railway Trail and Enhanced Connections
  -  Potential Public Art Location

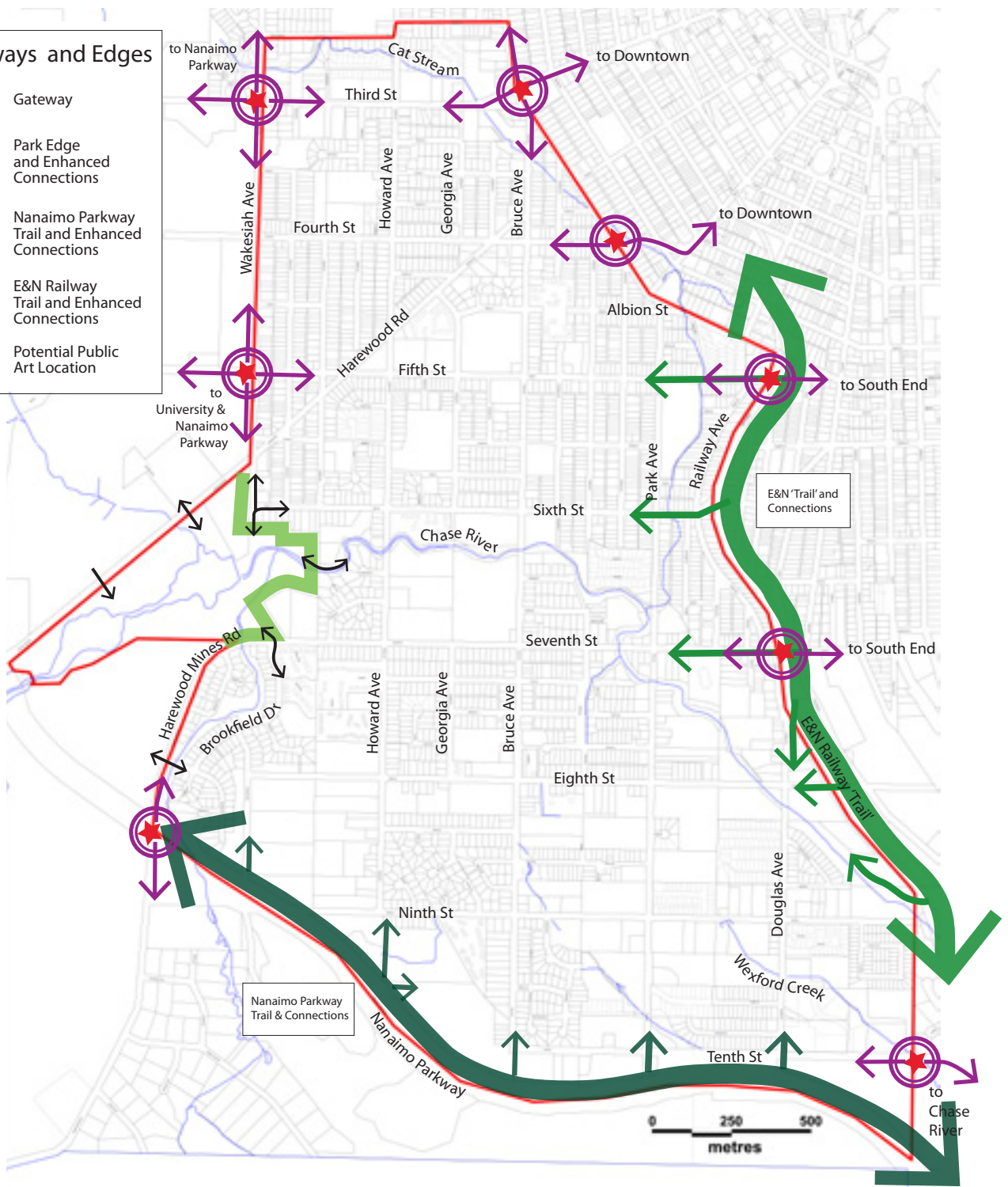
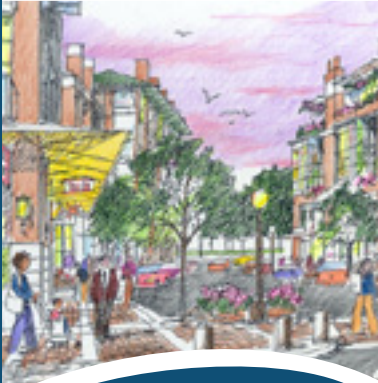


Figure 6 - Gateways and Edges



## 1.4 Views and Topography

Harewood's topography forms a natural bowl, with the highest elevations around the southwestern and southern perimeters, lowest elevations towards the centre of the study area, and a ridgeline along the eastern edge of the area that is partially occupied by the E & N Railway line. The Chase River cuts through the centre of the area, and is joined by tributaries including the Cat Stream from the north and an unnamed creek from the south. These watercourses and a small wetland just south of Eighth Street occupy the lowest elevations of the area, and are a major feature of the natural topography. Mount Benson forms a dominant backdrop to the west.

As a result of this distinctive topography, there are panoramic views across the area from the southern and western edges towards both the water and the north. There are also panoramic views toward Mount Benson to the west down several of the east west streets, and views down several north south streets right across the neighbourhood. These views and topographic context contribute significantly to Harewood having a unique sense of place. Protecting and celebrating this natural context and the associated views are important aspirations for the Harewood Neighbourhood Plan.

### Key Strategies

- 1 Maintain all street end views as noted, with new development set back and / or stepped.
- 2 Develop public view opportunities at natural high points, such as at Howard Avenue and Wharton Street; Fourth Street and Bruce Avenue; and Seventh Street and Short Avenue.
- 3 Preserve panoramic views from high points in the southwest sector of the neighbourhood.
- 4 Optimize views to Mount Benson and other surrounding features by encouraging built forms that respond to the topography and views with stepped profiles, setbacks and angled rooflines.
- 5 Develop rainwater management strategies on steeply sloped streets including rainwater retention areas, swales, rain gardens and rainwater storage areas.

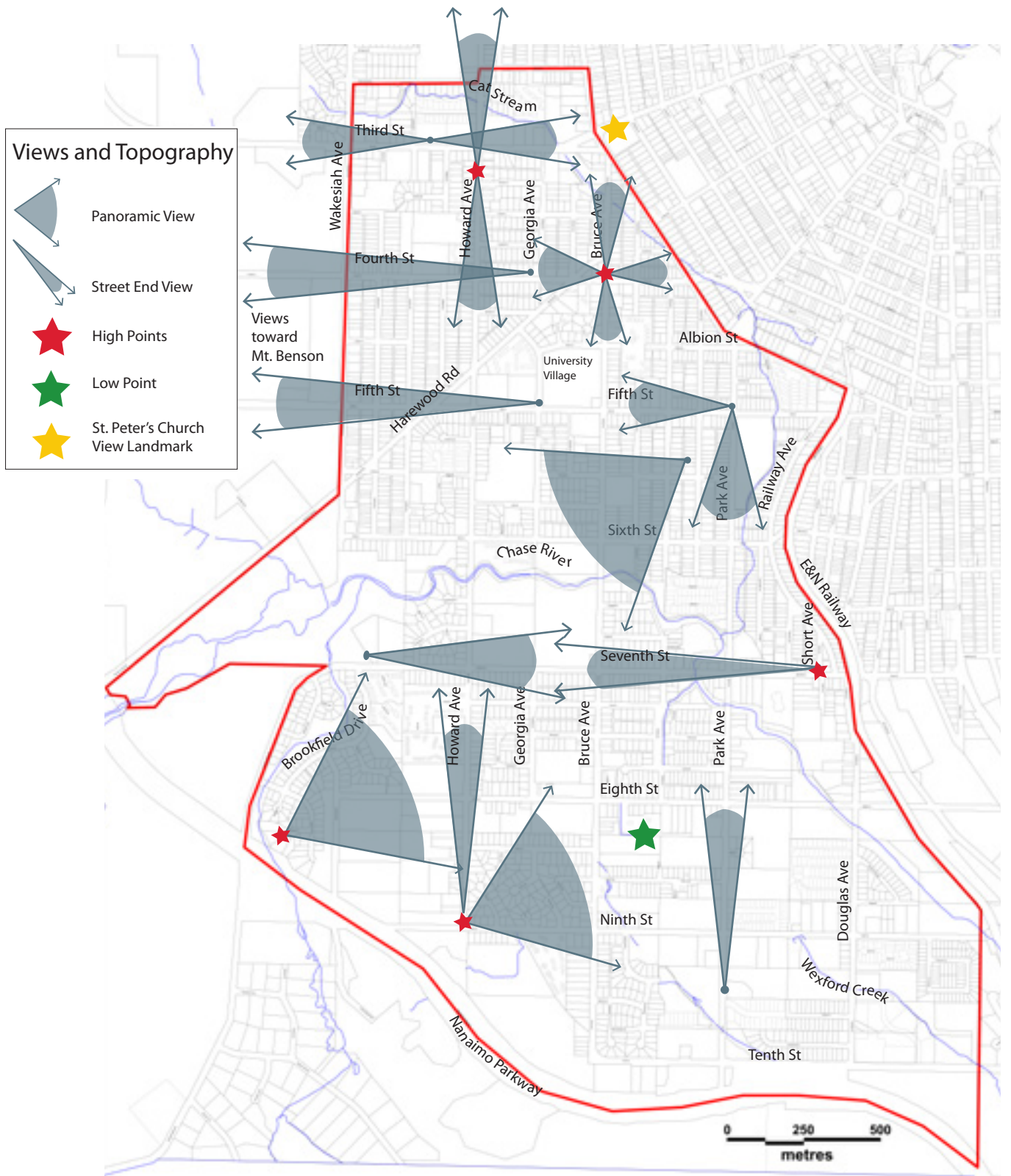
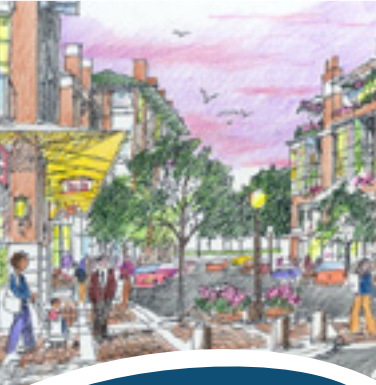


Figure 7 - Views and Topography

## 1.5 Pedestrian Network and Open Space










A fine grained network of enhanced pedestrian routes on streets and trails are the foundation of a sustainable neighbourhood. This pedestrian network connects places to live, work, shop and play, which reduces dependence on automobiles and offers residents an alternative mode of travel. The proposed pedestrian network also optimizes the existing natural assets in the neighbourhood, particularly access to and along the network of creeks and rivers.

The proposed open spaces build on the existing network of parks and open space. The aim is to better integrate this network and also provide more continuous and consolidated natural open space along watercourse corridors such as the Chase River and Cat Stream.

### Key Strategies

- 1 All streets to have sidewalks on both sides of the street, with priority given to major roads.
- 2 Sidewalks to be designed to current urban standards, with street trees, landscape, rainwater management, adequate pedestrian lighting and universal access elements such as curb drops at intersections, bulges, and lit crosswalks.
- 3 Streets identified on Figure 8 as a “Pedestrian Priority Zone” to be designed to an enhanced pedestrian standard, with wider sidewalks and more intense amenities such as landscape, street furniture and public art.
- 4 Develop additional, and enhanced, connections to the Nanaimo Parkway Trail at the south end of all north-south streets, and to the future E & N Railway Trail.
- 5 Secure, through purchase, development approvals negotiation, easements, or rights of way, or other methods, a number of strategic land parcels to create land reserves and additional open space along watercourses, and to consolidate existing disconnected parks such as along Park Avenue between Fifth Street and Seventh Street; along the unnamed creek south of Eighth Street between Bruce Avenue and Park Avenue; along the Cat Stream; and along the Chase River.
- 6 Streets to be designed as per Section 8.1 General Street Design.

### Pedestrian Network Plan

-  Pedestrian Priority Zones
-  Major Pedestrian Routes
-  Existing Multiple Use Path
-  Potential Multiple Use Path
-  Existing Pedestrian Path
-  Potential Pedestrian Path
-  Existing Traffic Signal
-  Potential Traffic Signal
-  Potential Mid Block Pedestrian Crossings
-  Existing School
-  Parks + Open Space
-  Commercial Node
-  Existing Open Space
-  Proposed Open Space

All roads to be developed with sidewalks on both sides of the street.

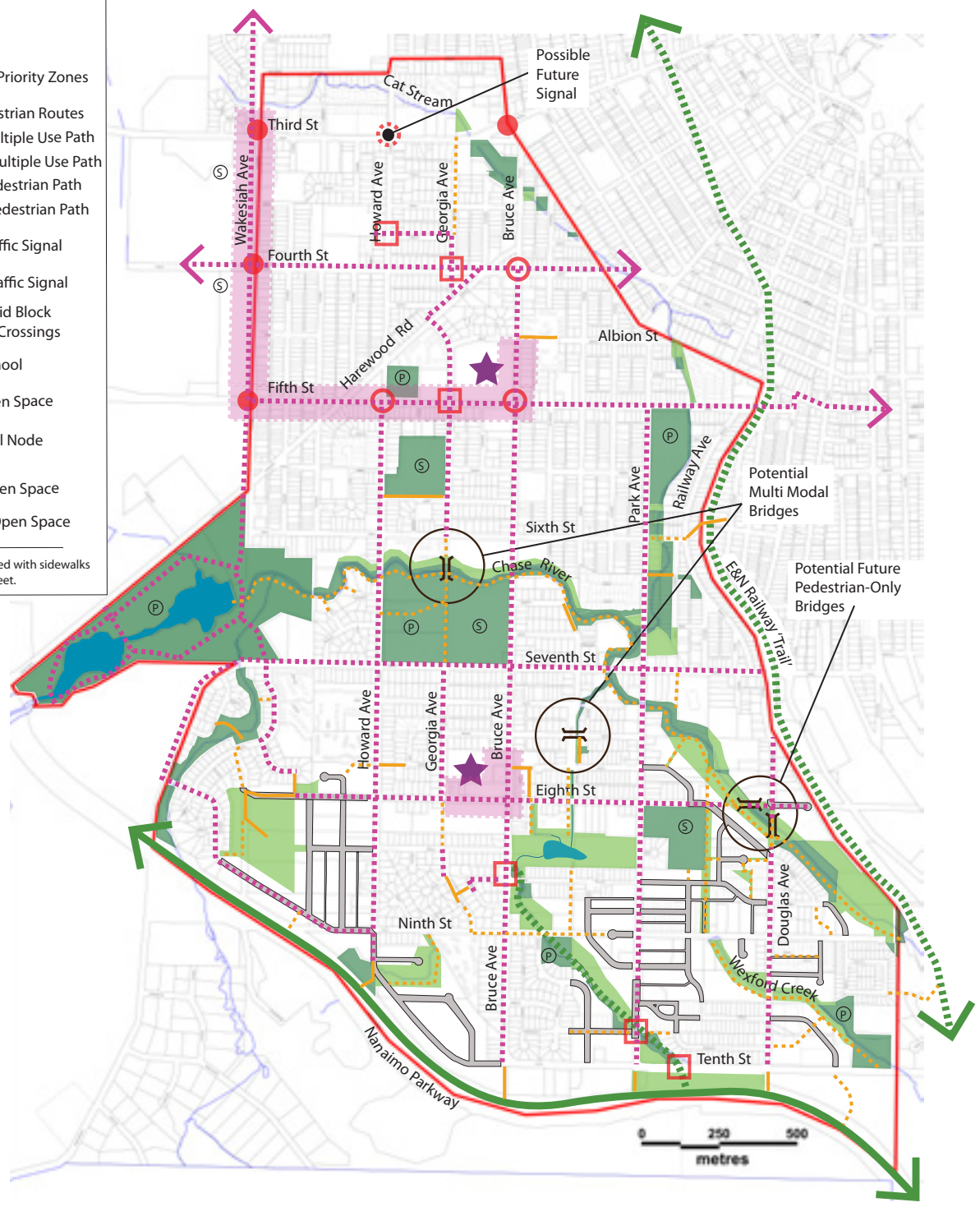
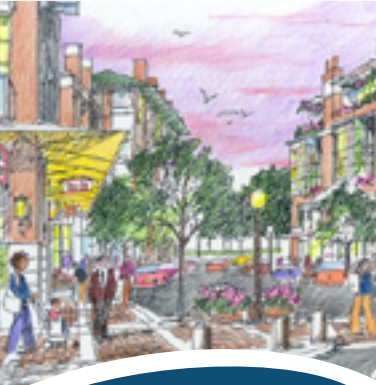


Figure 8 - Pedestrian Network and Open Space

## 1.6 Watercourses



The existing watercourses that flow through Harewood are a major natural feature of the neighbourhood and an important determinant of land use planning and urban form. They are also under threat from insensitive development, polluted stormwater runoff, and neglect.

Consistent with the Official Community Plan's goal to *protect and enhance our environment*, the proposed watercourse strategy aims to protect and enhance the existing watercourses as a key natural asset, and to optimize the natural, healthy functioning of the watercourse system. It also supports a more sustainable neighbourhood plan, and optimizes the responsible use and enjoyment of these watercourses as a neighbourhood asset with an expanded trail network. The proposed trail network optimizes access to and along the network of creeks and rivers, while being located and designed to minimize disturbance to riparian corridors and protect sensitive habitat.

The proposed open spaces along the watercourses build on the existing fragmented network of parks and open spaces. The aim is to provide more continuous and consolidated protected open space along watercourse corridors such as the Chase River and Cat Stream. These open spaces serve both a protective function and a recreational function.

### Key Strategies

#### **Protect and enhance existing watercourses with increased development setbacks along riparian corridors**

- 1 Setbacks along the Chase River and Cat Stream should be increased where they are substandard, as measured by current provincial and federal government fisheries and watercourse protection regulations and standards. This may mean negotiating extended 'leave strips' with private landowners whose property includes or abuts the riparian zone.
  - Use the Official Community Plan Natural Hazard Lands and Steep Slope Development policies to manage and / or restrict development in these areas
  - Apply the City's Watercourse Development Permit Guidelines

#### **Expand and connect the open space system along watercourses**

- 2 Secure additional open space along and beside watercourses, through a variety of means including:
  - Strategic sites acquisition
  - Development approvals agreements
  - Development Cost Charges (DCCs)
  - Community Amenity Contributions (CACs)
  - Negotiated easements and / or rights of way with land owner



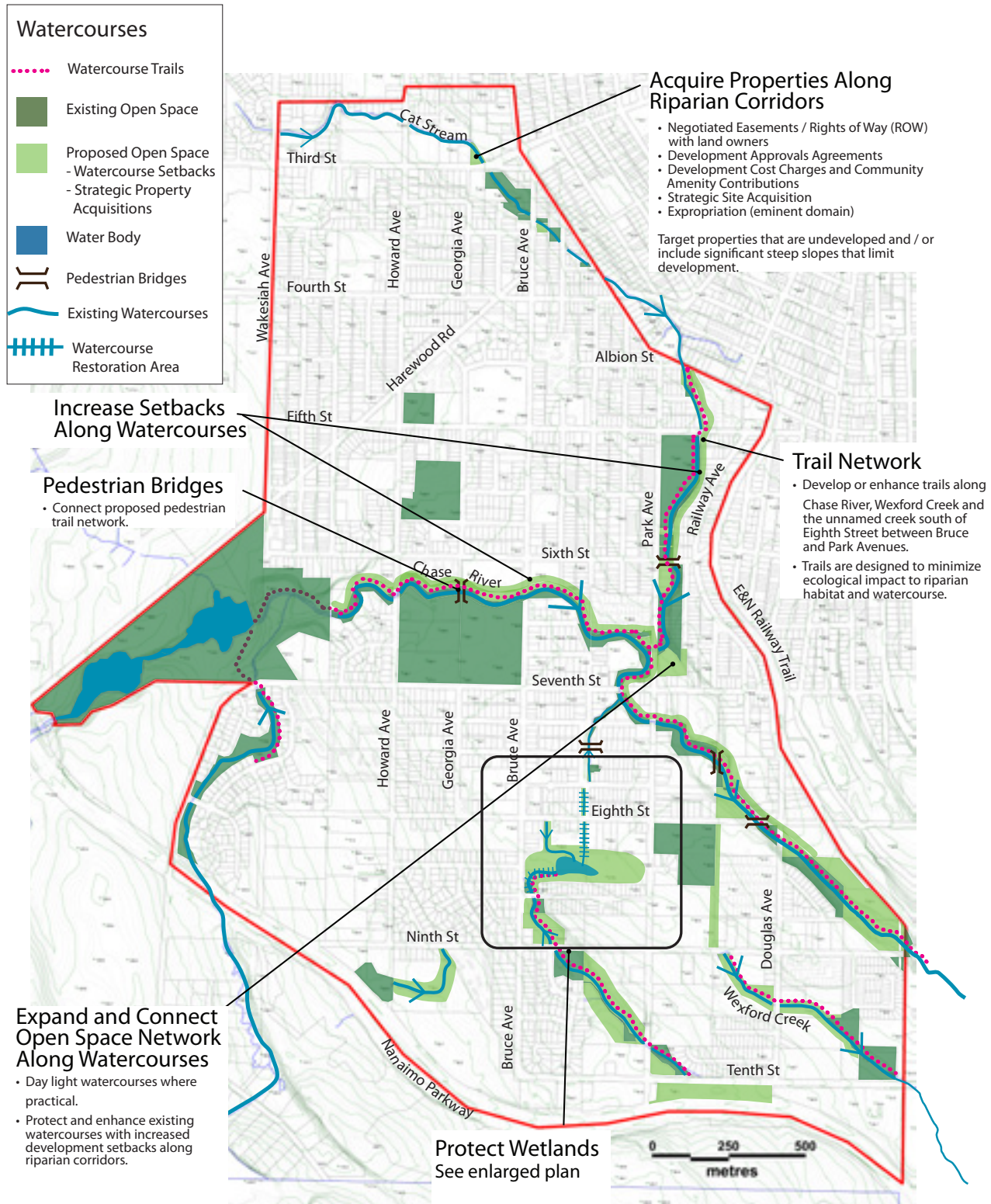
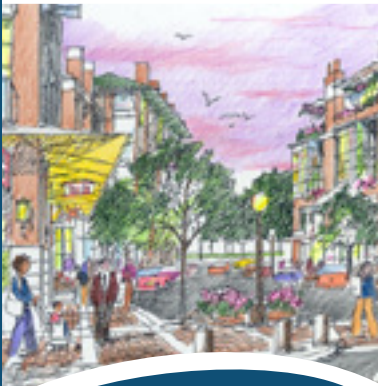


Figure 9 - Watercourses



- 3 Target properties that are undeveloped and / or include significant steep slopes that limit development.
- 4 Target properties that expand the existing fragmented network of parks and open spaces and whose acquisition will provide more continuous and consolidated open space along watercourse corridors. Figure 9 identifies proposed open spaces.

### Daylight watercourses where practical

Parts of the natural creek system that feed into the Chase River have been channelled and / or buried underground in pipes or culverts.

- 5 Figure 10 identifies the unnamed tributary creek that flows northwest from Tenth Street toward Bruce Avenue, and identifies a section north of Ninth Street where it should be daylighted and redirected toward the existing wetland located east of Bruce Avenue and south of Eighth Street. This section of the creek has been disturbed and / or buried, and restoring it to flow into the wetland would be a major enhancement of the area's natural systems.
- 6 Figure 10 also identifies a potential new channel for draining the wetland to the north so that the creek rejoins the existing creek channel along Stirling Avenue at Deering Street, where it flows into the Chase River. Extending this channel between the wetland would restore this creek as a continuous, daylighted watercourse from Tenth Street to the Chase River.

### Protect wetlands

As noted, there is an existing wetland basin that straddles several properties between Bruce Avenue and Park Avenue, and between Alpen Way and Eighth Street. Other than the Chase River valley floor itself, this is the lowest point in the entire Harewood neighbourhood. It is most likely that this wetland was formed as a result of the surrounding topography being modified for development purposes, which in turn disturbed the original creek watercourse through this area. Water from the surrounding area now drains into this basin.

This wetland should be protected through acquisition of the land parcels surrounding it. These lands have historically been used for agriculture and are still largely undeveloped. There is an opportunity for the City to acquire these land parcels via the Parks Master Plan, both to protect the wetland and creek, and to preserve open space in this area for urban agriculture and other public uses.

- 7 Secure a land reserve to protect the wetland basin at the lowest elevation point in the neighbourhood (land parcels between Bruce Avenue, Park Avenue, Eighth Street and Alpen Way). Figure 10 generally identifies the subject land parcels.

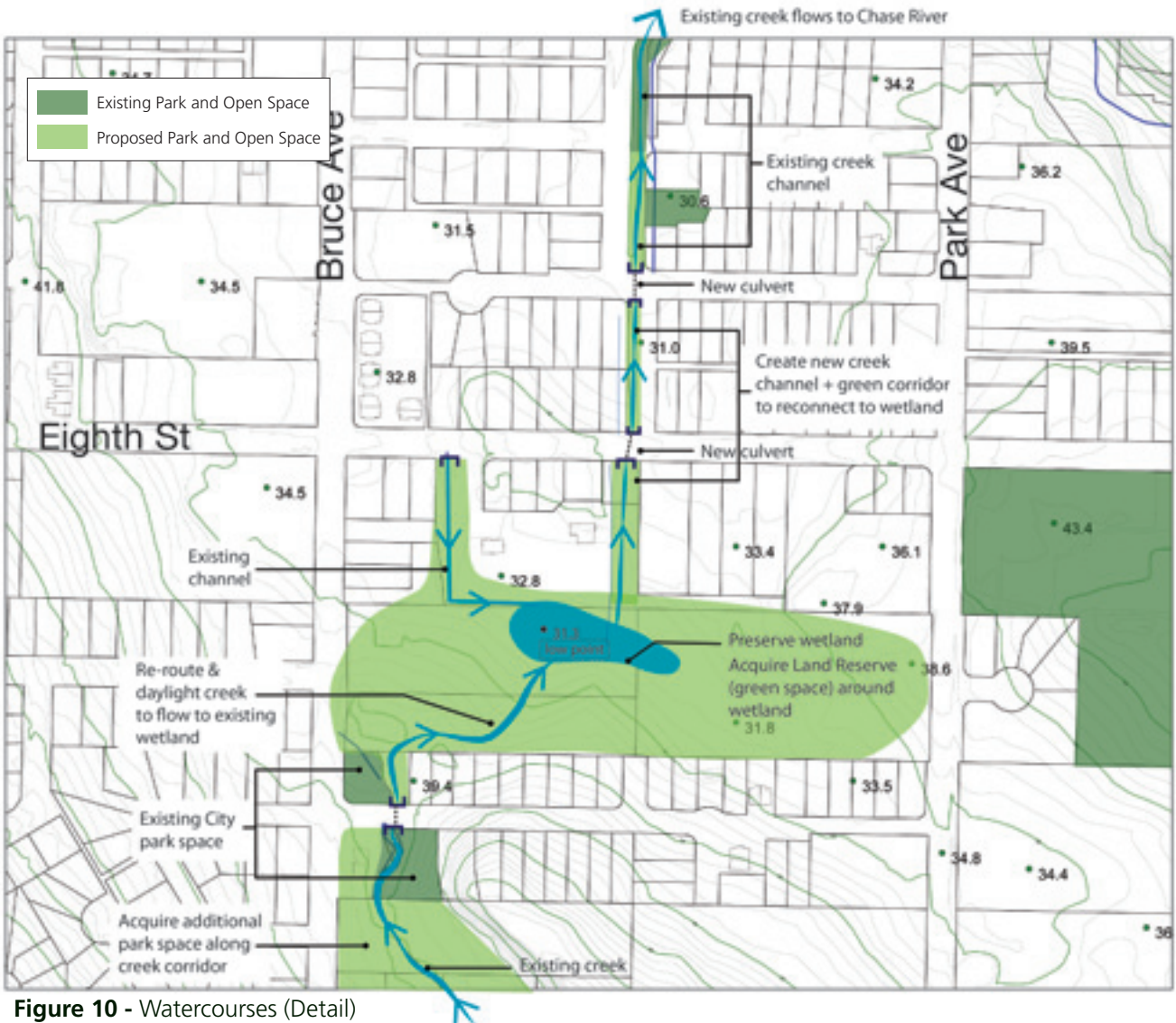


Figure 10 - Watercourses (Detail)

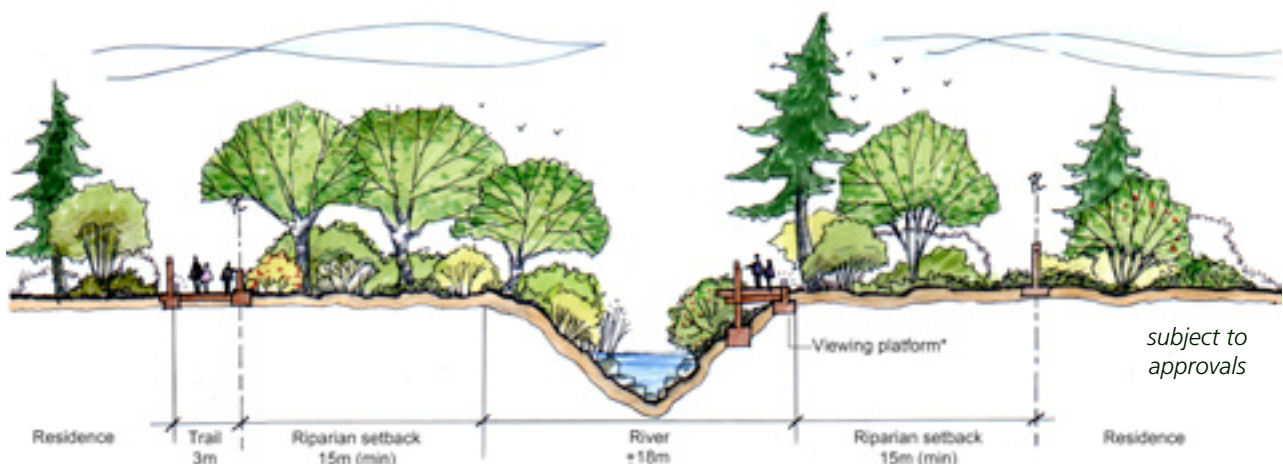
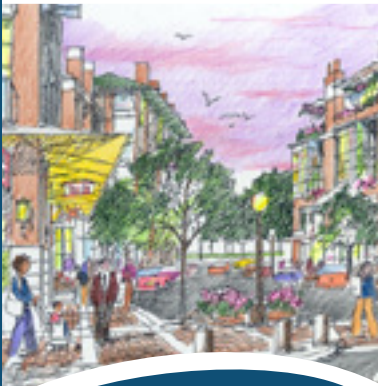


Figure 11 - Generic Cross Section Showing Watercourse Trails



**Provide an extended watercourse trail network, designed to minimize environmental impacts**

- 8 Develop or enhance public trails along watercourses including the Chase River, Wexford Creek and the unnamed creek south of Eighth Street, between Bruce Avenue and Park Avenue.
- 9 Design and construct watercourse trails to minimize impacts on riparian habitat and watercourse.

Figure 11 identifies a generic approach to designing and locating such trails. It shows two possible alternative locations for watercourses trails:

- Outside the riparian zone setback but within privately owned property (easement assumed); or
- within the river bank / riparian zone setback (subject to approvals).

The actual location and design of all watercourse trails would be subject to approvals.

**Identify locations for pedestrian, multi modal bridges over watercourses, to integrate the proposed pedestrian trail network**

- 10 Develop new pedestrian, multi modal bridges across watercourses where pedestrian routes are proposed, such as along Eighth Street, Douglas Avenue, and Georgia Avenue rights of way.

**Better manage rain water runoff into creeks**

- 11 Develop enhanced systems to better manage rain water and stop it from draining directly into creeks or the Chase River. This may include:
  - Retention ponds
  - Bioswales
  - Filtration systems to filter out heavy pollutants
  - Pervious paving
  - Raingardens
  - Constructed wetlands
  - Street curbs and gutters with separated rainwater facilities
  - Vegetation and tree planting to absorb water and stabilize slopes

**Restoration Projects and Education**

- 12 Coordinate restoration projects, such as invasive species removal and native vegetation planting, with the City's Parks, Recreation and Culture Department.
- 13 Educate residents about the importance and uniqueness of the rivers and creeks in the neighbourhood through programming and interpretive signage.

**Colliery Dam Park**

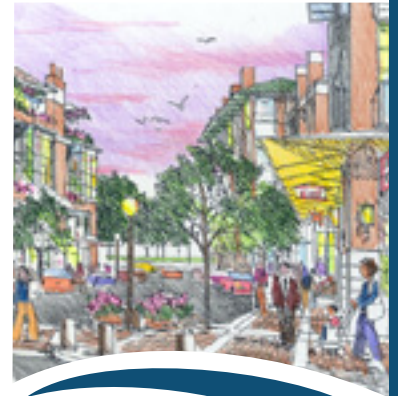
- 14 Continue works to address safety, recreation, and environmental issues for Colliery Dam Park.

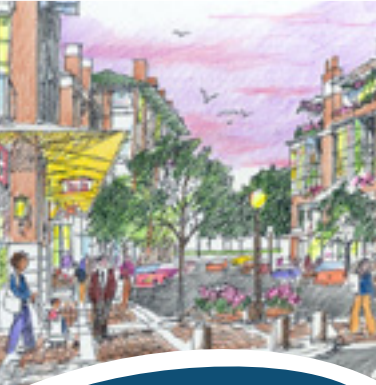
## 1.7 Road Network

The Harewood road network is based on a hierarchy system that reflects the function of each roadway, in accordance with City of Nanaimo classifications. This network includes major roads, such as Third Street, and minor roads, such as Eighth Street. The Harewood road network also provides the fundamental public space framework, connecting people to each other and to destinations. The street network is not just about transportation but also about the movement of people and goods, and providing public space for interaction.

### Key Strategies

- 1 Provide an open, connected and continuous grid or modified grid hierarchical road system. Well connected street networks improve mobility by allowing people to travel more directly, making destinations more accessible by walking, and enlarges the catchment area of transit stops. Highly connected street networks have been shown to reduce vehicle miles travelled, and reduce traffic congestion and vehicle delay by diffusing traffic across the larger street network.
- 2 Connect and provide access to and between communities in the city, major centres of activity, and neighbourhoods of all types, as well as recreational and cultural facilities.
- 3 Form a grid like or modified grid pattern of continuous streets except as precluded by topographic features, while avoiding adverse impacts to natural resource areas.
- 4 Major roads should be interconnected with specified distances between intersections to provide choices of routes to reduce travel distances, to promote the use of transit, bicycles and walking, and to efficiently accommodate utility needs. In general, major roads (Arterial, Major Collector and Minor Collector) should have no more than 400 metres, or 1/4 mile spacing, in urban areas.
- 5 Provide pedestrian facilities so block lengths in less dense areas (suburban or urban) do not exceed 200 metres (preferably 60 metres to 120 metres) and relatively direct routes are available. In Corridors, street block length should not exceed 120 metres (preferably 60 metres to 90 metres) to support higher densities and pedestrian activity.
- 6 Intersperse major roads with a system of Neighbourhood Collector Roads, which distributes trips more efficiently, keeps short local trips off the arterial system, and provides multiple route choices for transit, pedestrians, drivers and bicyclists.
- 7 Build network capacity and redundancy through a dense, connected network rather than an emphasis on high levels of vehicle capacity on individual Arterial or Major Collector roads. This approach (more streets rather than wider streets) ensures the network and roadway facilities support other objectives such as improving pedestrian connections, and supports higher density of adjacent development.





- 8 Encourage small, walkable blocks with no cul de sacs, unless these are required to avoid environmental or other impacts. Loop roads are preferred over cul de sacs to ensure two entry / exit points to development parcels.
- 9 Minimize direct property access directly onto Arterials and Major Collector Roads, with the development of raised medians to prevent left turns at existing driveways.
- 10 Provide rear lane or public 'back access' roads for development fronting Arterial and Major Collector Roads, and particularly in the *Harewood Village*; this will improve the pedestrian environment by reducing driveway crossings and improve safety and smooth traffic operations on these busier streets.
- 11 Accommodate emergency vehicles, providing multiple routes to reach any block.
- 12 Provide signalized crossings to encourage use of walking, bicycles and transit.
- 13 Communicate the intended functions of the street through both design characteristics and appearance.
- 14 Allow for future provision of left turn lanes on Arterial and Major Collector Roads at intersections with other Arterials, Major Collectors and Minor Collectors to ensure smooth traffic operations.
- 15 Road classifications and road rights of way widths should be pursued, as set out in the Harewood Neighbourhood Plan Transportation Plans and Policies, 2013, and Section 8 of this Neighbourhood Plan.

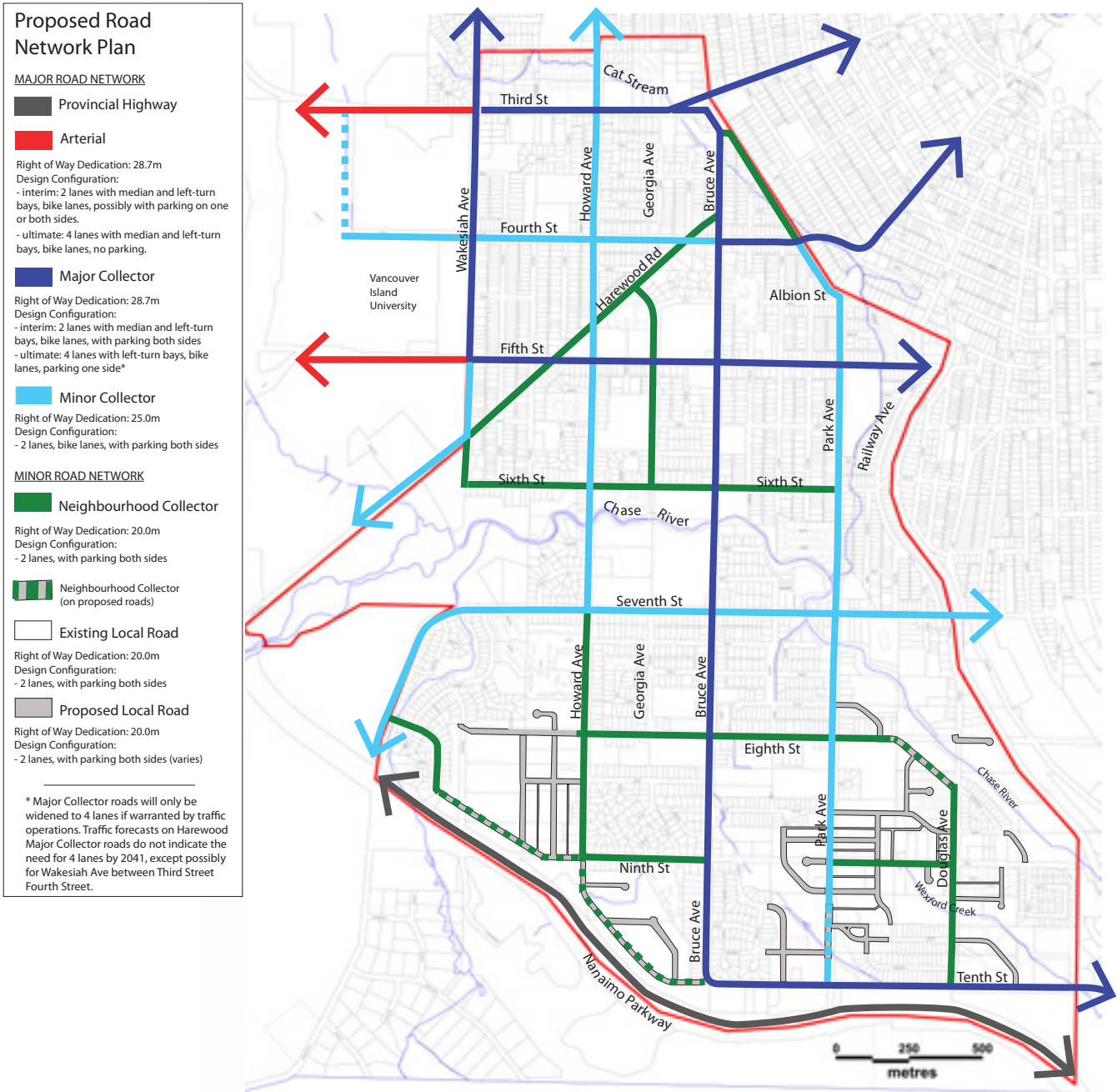
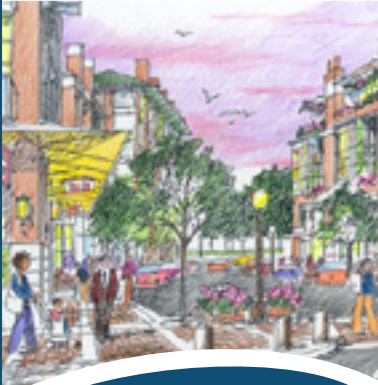


Figure 12 - Road Network



## 1.8 Laning + Traffic Control

Since traffic growth on Harewood streets is expected to be fairly modest, the future laning and traffic controls required in the neighbourhood are not anticipated to be greatly different from today. All streets are expected to operate reasonably well with two travel lanes, although Wakesiah Avenue, between Third Street and Fifth Street, may be close to warranting four travel lanes by 2041. Fifth Street west of Bruce Avenue in the proposed *Harewood Village* will also be relatively busy during peak demand periods. In these busier blocks, the desire for efficient traffic operations should be balanced against the overall objective to create walkable commercial street frontages with slow moving traffic; that is, the mobility function of these streets should be less important in these blocks where the adjacent land uses and context of the street will be paramount to meeting the objectives of the Harewood Neighbourhood Plan.

### Key Strategies

- 1 New traffic signals are anticipated to support good traffic operations by 2041 in the following locations:
  - Fourth Street and Bruce Avenue
  - Fifth Street and Howard Avenue
  - Fifth Street and Bruce Avenue
  - Seventh Street and Bruce Avenue
  - Eighth Street and Bruce Avenue
- 2 A new traffic signal may be appropriate for the Third Street and Howard Avenue intersection due to difficult vertical geometry and crash history. New stop controls are anticipated to be required at Ninth Street and Howard Avenue, and at Tenth Street and Bruce Avenue, as redevelopment occurs and traffic volumes increase.
- 3 New left turn bays may be required at Ninth Street and Bruce Avenue, and at Fourth Street and Park Avenue. Other than these locations, all existing left turn bays, and the storage provided by these left turn bays, should be sufficient to accommodate peak queues. All Arterial and Major Collector intersections with other Arterial and Collector roadways to be developed with left turn bays, with storage for at least three vehicles or one bus (15 metres). On Minor Collector Roads, separate left turn bays are generally not required due to lower turning volumes.
- 4 The ultimate configurations of both Arterial and Major Collector Roads include provision of a raised landscape median to restrict mid block driveway access, if present, to right-in-out movements only. These medians necessitate development of a back road access system. In the case of *Harewood Village* and Wakesiah Avenue, between Third Street and Fifth Street, a rear lane system will be needed to support the planned street front commercial development.



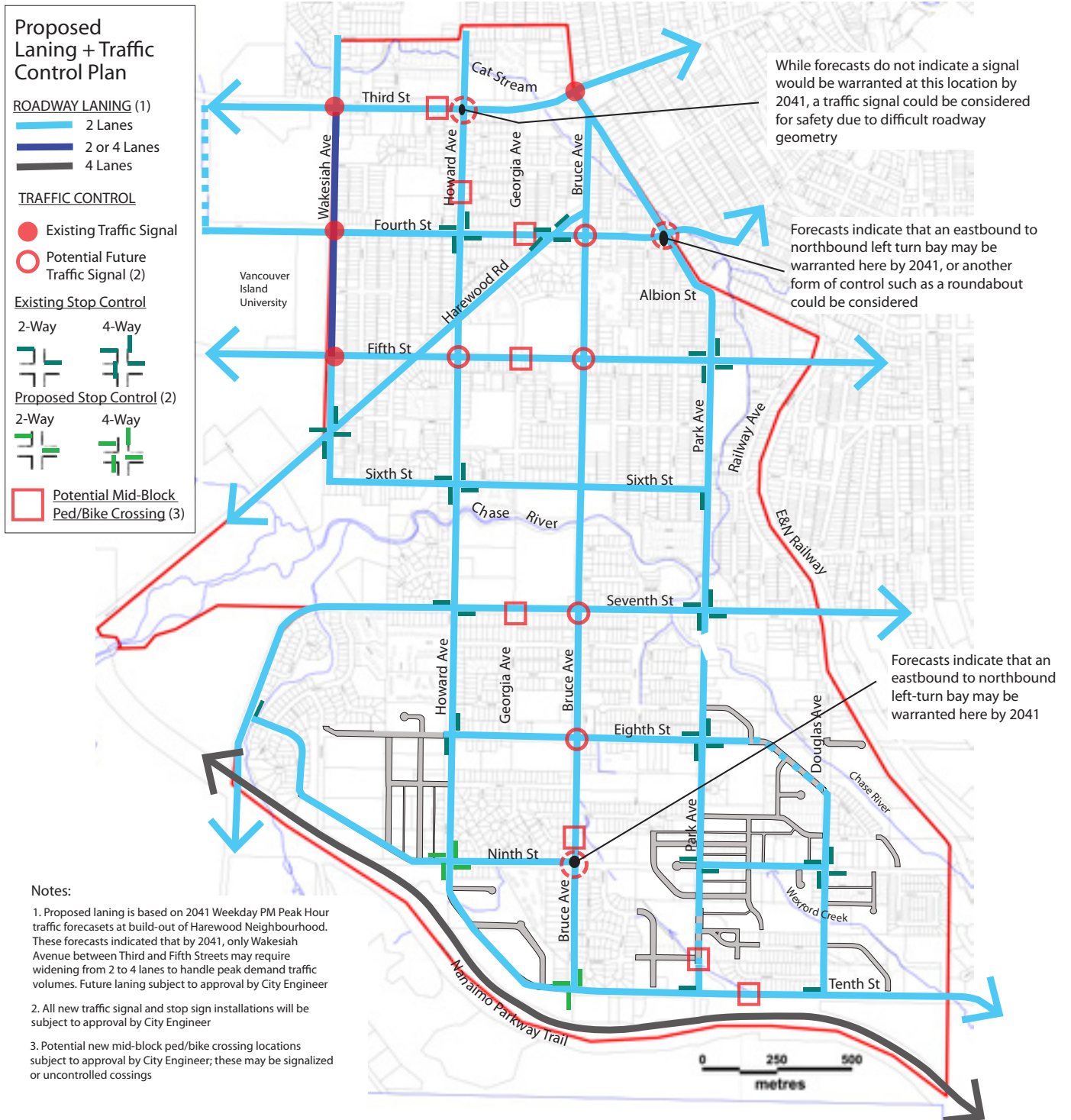
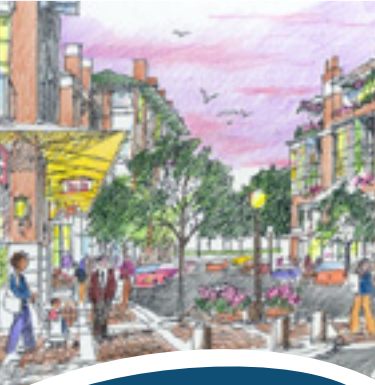


Figure 13 - Laning + Traffic Control

## 1.9 Bicycle Network



A comprehensive network of bicycle facilities that connect key destinations in the community, transit and the city wide cycling network, can expand cycling as a viable mode of transportation across the neighbourhood and to nearby major trip generators like Vancouver Island University. The Harewood bicycle network will include a system of bicycle facilities with parallel routes, providing direct connections to major trip generators such as schools, retail districts and parks. Bicycle facilities will include on street bike lanes, shared lanes, off street paths and bikeways on traffic calmed streets with low motor vehicle volumes.

### Key Strategies

- 1 Develop a complete network of on street bicycle lanes that connect key destinations within and beyond the neighbourhood, including Harewood Village, the Nanaimo Aquatic Centre, schools, Vancouver Island University, and recreation areas.
- 2 Plan all Arterial, Major Collector and Minor Collector roads to have 1.8 metre wide bicycle lanes, with supporting pavement markings and signage.
- 3 Create a neighbourhood street bikeway on Georgia Avenue, including a multi use path across Harewood Centennial Park, and a multi modal bridge across Chase River. This route will utilize rear lanes as well as sections of Georgia Avenue between Fourth Street and Eighth Street. Over time, the route could be developed into a full greenway street with traffic calming features to limit / slow vehicular traffic.
- 4 Create a neighbourhood street bikeway on Hillcrest Avenue, one block east of Wakesiah Avenue between Harewood Road and Foster Street, to serve as an alternate parallel route to the Wakesiah Avenue corridor.
- 5 Create a neighbourhood street bikeway on Harewood Road, between Wakesiah Avenue and Bruce Avenue. This roadway, offers direct connections between several important neighbourhood destinations, and there is an opportunity to develop a bikeway with significant traffic calming measures. The angled intersections also offer opportunities for extra right of way space that may be developed as public spaces along the bikeway route, and their closure or modification could contribute to vehicle safety as well.

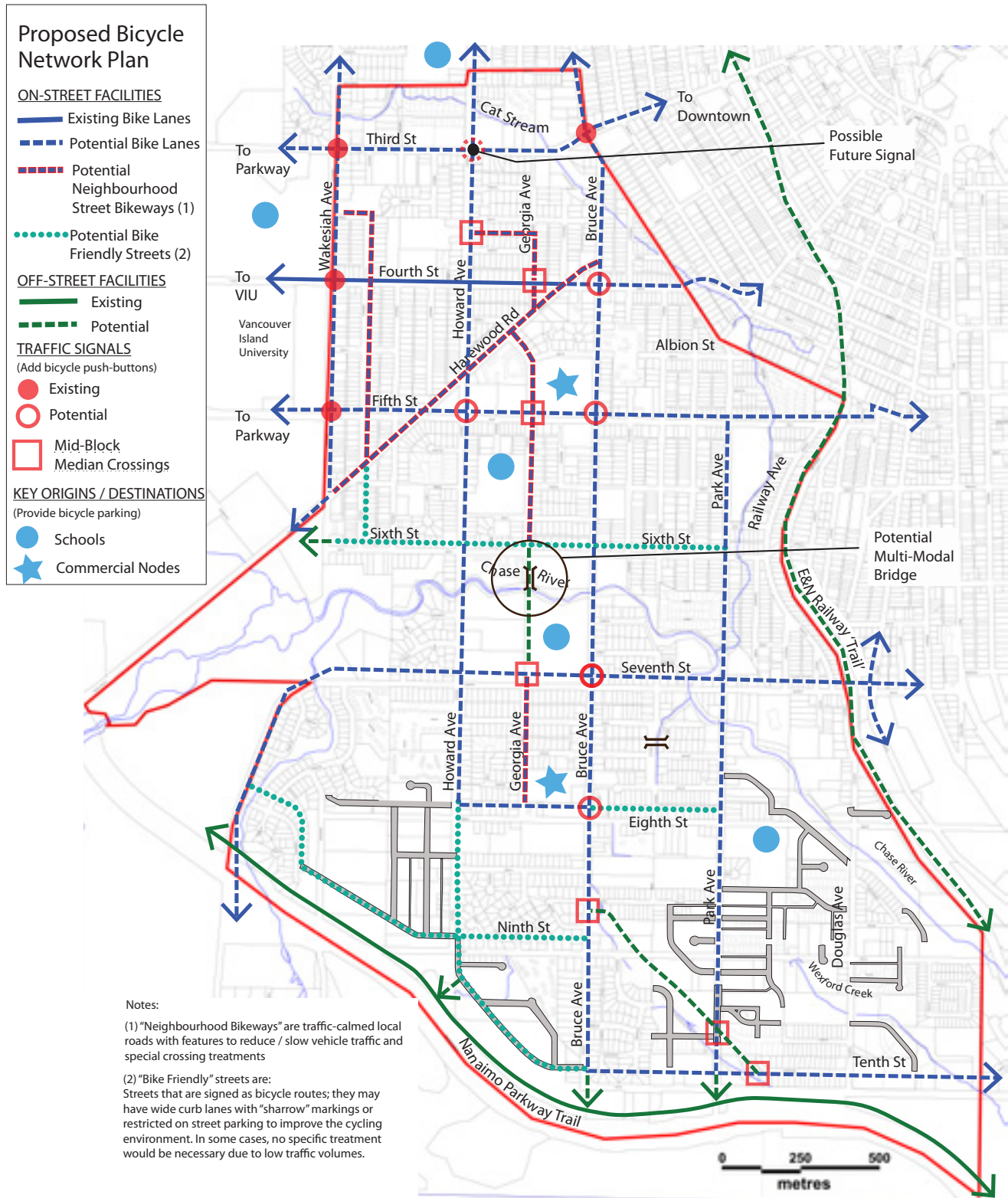
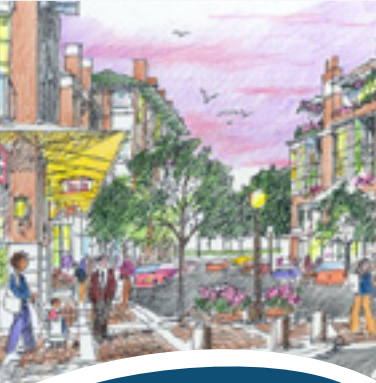


Figure 14 - Bicycle Network

## 1.10 Transit



For the City of Nanaimo, including Harewood, transit service is operated through the Regional District of Nanaimo. Various components of the transit system include routes, exchanges, timing points, and stops. The first three elements are addressed here.

The 2006 Census indicates only 1.5% of Harewood work trips are made by transit, compared to 3.5% in the entire City. Transit routes are planned to balance service coverage and ridership potential. In lower density suburban neighbourhoods, such as Harewood, it is a challenge to meet accessibility targets (typically 450 metre maximum walking distance to a bus stop) while also providing relatively frequent service that might attract higher levels of ridership (typically frequencies in the order of 10 minutes or less during peak periods).

With the planned densification of the Harewood Neighbourhood along the Third Street, Wakesiah Avenue and Bruce Avenue Corridors, there is an opportunity to focus transit services on these routes. With the planned new development areas in South Harewood and the south end of the City, there will be an opportunity to create new transit connections to the south that are not currently available.

### Key Strategies

- 1 Proposed changes to existing routes are summarized in Figure 15.
- 2 Maintain and enhance existing exchanges at Vancouver Island University, and at Fifth Street and at Bruce Avenue. Provide a new exchange near Tenth Street and Bruce Avenue.
- 3 Locate and design bus stops as laid out in Section 8.1 General Street Design.

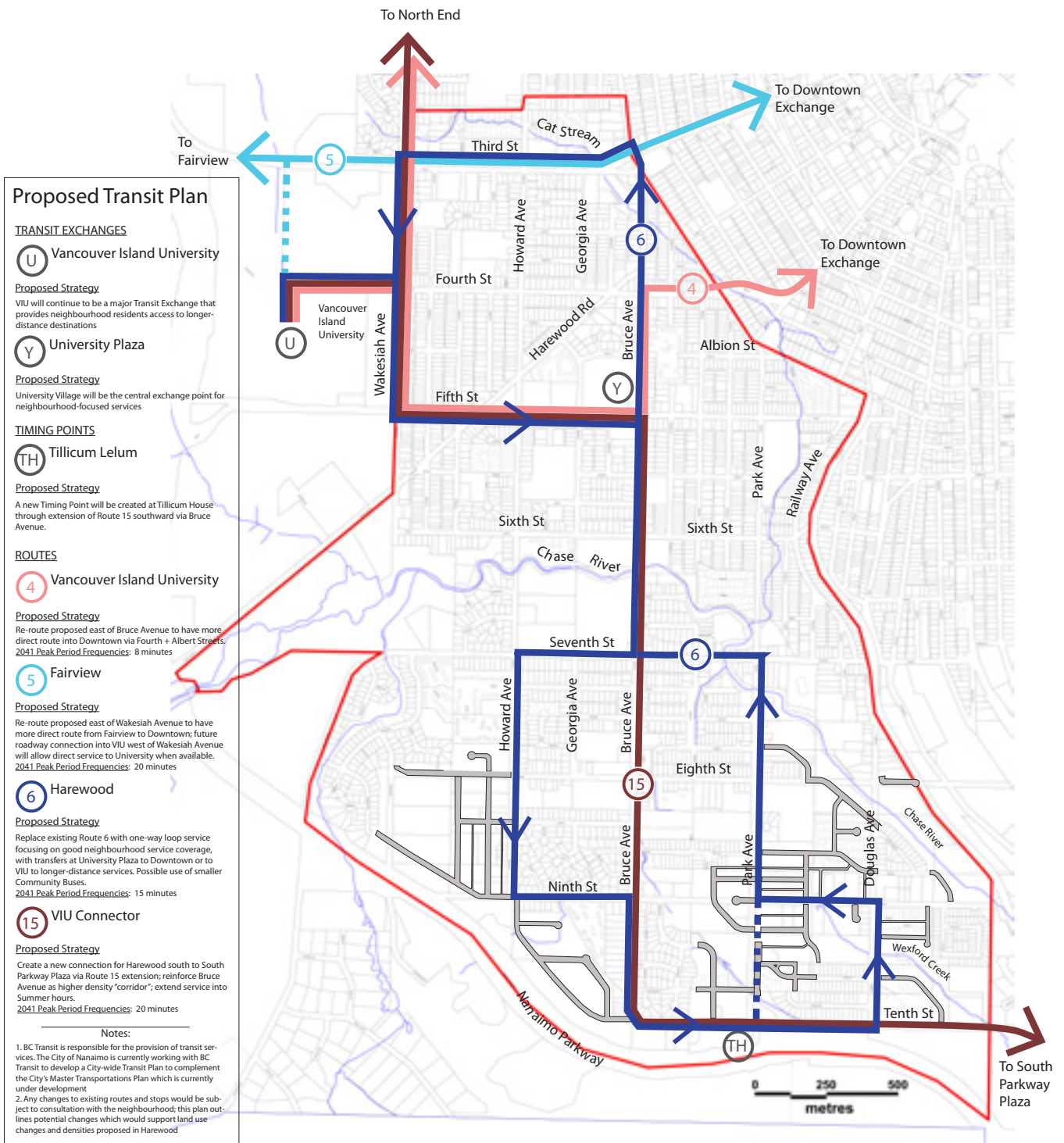
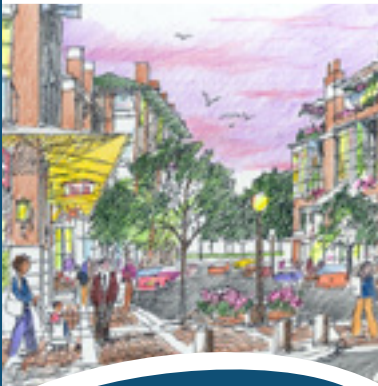


Figure 15 - Transit

## 2 Urban Design Guidelines



The following Design Guidelines apply to the Harewood Neighbourhood Plan area.

The intent of these Guidelines is to foster better design, reduce negative impacts on competing uses, and enhance the neighbourhood's urban form and public realm. The Design Guidelines are also intended to ensure that new development supports the neighbourhood context and the City's move toward more sustainable forms of urban settlement.

These Design Guidelines provide recommendations for urban design, street design, parking design and building design, as well as Sustainability and Crime Prevention Through Environmental Design (CPTED).

Building Design Guidelines address both Corridor designated building types and Neighbourhood designated building types. A series of illustrations and images respecting various building forms help visualize the proposed urban form and provide quick reference models for the intent of the Design Guidelines.

- General Building Design
- Sustainability
- CPTED
- View Corridors
- Roof Forms
- Off Street Parking and Loading
- South Harewood Village
- Mixed Use Development Up to 6 Storeys
- Residential Development Up to 6 Storeys
- Neighbourhood Development

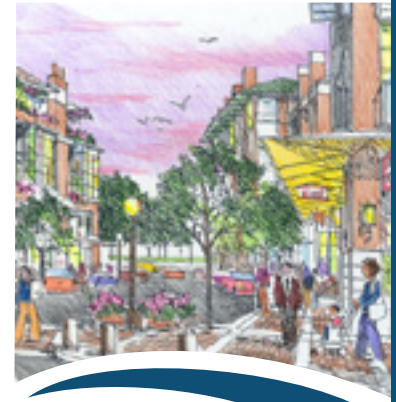
Street Design Guidelines address general street design for the area, as well as covering several of the key streets in the neighbourhood.

- General Street Design
- Enhanced Local Streets
- Bruce Avenue
- Fifth Street
- Fourth Street
- Tenth Street
- Third Street
- Wakesiah Avenue

### 3 Building Design Guidelines

#### 3.1 General Building Design

Development within the Corridor designation in Harewood will focus on a range of housing forms, mixed use developments (including commercial uses), as well as community amenities and services. Building design should demonstrate best practices in sustainability and contribute to a walkable, cyclable and transit oriented neighbourhood. The form and character should reflect Harewood’s vibrant community and rural roots. Developments up to six storeys are supported within the Corridor. This document includes guidelines for a range of multiple family and mixed use typologies permitted within Corridor designated areas.



#### Character and Neighbourhood Identity

Harewood has a long and rich history founded on coal mining and agriculture. The pastoral and natural landscape contributes to Harewood’s identity and character, and should be preserved and enhanced as much as possible.



Areas of open landscape contribute to Harewood’s rural identity.



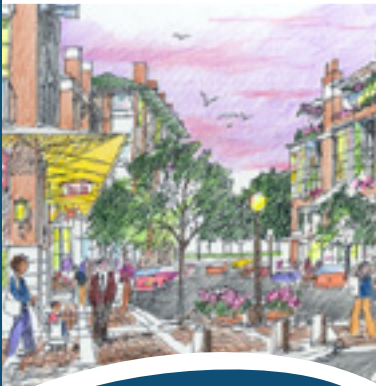
Harewood’s first European settlers set the foundation for its development as a community.



Tillicum Lelum Aboriginal Society’s contemporary architecture of Salish origin evokes Harewood’s connection with local First Nations communities.

The form and character of development should reflect Harewood’s roots and bright future. Contemporary architecture including passive solar design strategies and the use of natural materials is encouraged throughout these guidelines.



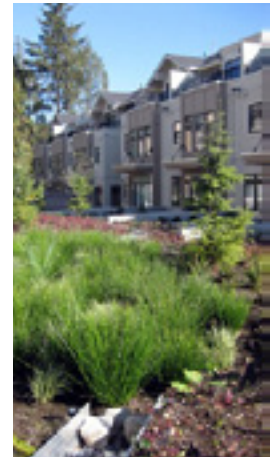
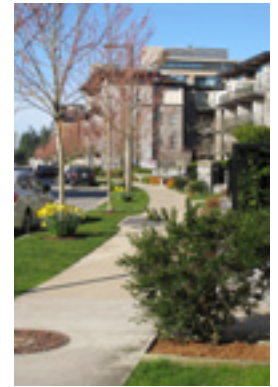


### 3.2 Sustainability

These design guidelines incorporate sustainable community design principles and best practices. The following is a summary of the basic design principles.

#### Rainwater Management

- 1 Minimize impermeable surface areas wherever possible.
- 2 Residential projects should have maximum 50% impermeable surfaces.
- 3 Mixed use and commercial projects to provide 50% effective site permeability, for example, infiltrate 50% rainwater on site.
- 4 Use of green roofs is encouraged.
- 5 No increased surface drainage off site.
- 6 To prevent unwanted rainwater entering the sanitary sewer system, all rainwater services such as roof leaders and perimeter drains should be inspected by a qualified professional, via Closed Circuit Television (CCTV), to determine condition and whether the service is emptying into the sanitary main. Where available, the storm service should be reconnected to the existing ditch or storm pipe located along the street.
- 7 No draining or filling of existing wetlands.



Beautifying the neighbourhood public realm can be achieved through good sustainable design practices that combine rainwater management and indigenous planting.

#### Landscape Design

- 8 Protect natural areas on and off site, including soil stabilization where necessary.
- 9 Retain and protect significant trees and plants where possible.



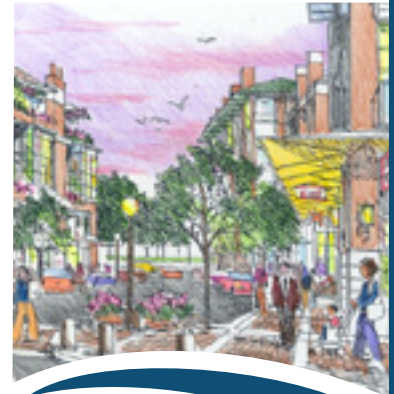
- 10 Enhance natural landscape along waterways to improve water quality, habitat and recreation potential.
- 11 Maintain and extend natural pathways or corridors through the neighbourhood as *habitat corridors* that allow animals and plants to access more resources, have a better chance at survival and maintain genetic diversity in their populations because of improved breeding opportunities.
- 12 Use of native landscape, edible landscape, and drought tolerant plants is encouraged.
- 13 Use of on site native soils is encouraged.
- 14 Consider optimal use of trees for shading and wind protection.
- 15 Provision of gardens, including on decks and roofs, is encouraged.

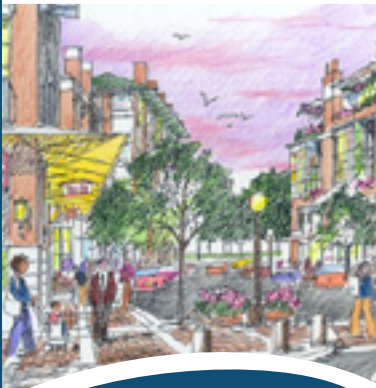
**Water Savings**

- 16 New and remodeled buildings should consider using at least 35% less water than code standard.
- 17 For development of existing lots, water lines and connections to the street should be inspected to ensure they are not leaking. Any necessary repairs should be completed as part of redevelopment of the site.
- 18 Avoid use of potable water for irrigation and other uses not requiring potable water. Collection of rain water, including use of rain barrels, is encouraged.
- 19 Use irrigation controls in gardens and landscaped areas.
- 20 Adhere to best practices standards, or a Green Building Standard, for low flow plumbing fixtures.
- 21 Consider providing rough in for grey water management system.



Residential and mixed use building design should integrate passive solar and rainwater management systems.





### Green Building Standards

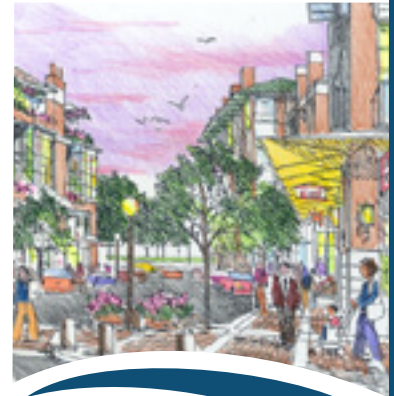
- 22 All new multiple family residential developments are recommended to be designed and built to standards outlined in the City's Sustainable Design Guidelines.
- 23 All multiple family residential developments and contractors are recommended to be registered in the Built Green BC program.
- 24 All residential renovation projects are strongly encouraged to comply with Built Green BC Silver Standard.
- 25 Mixed use and commercial projects are recommended to conform to the City's Sustainable Design Guidelines.
- 26 Use of green building products is encouraged.
- 27 Use best practices during construction to avoid runoff, dust, noise, and pollution that affects neighbours.
- 28 Use best practices to recycle waste during construction.
- 29 Developments in the Commercial Centre - Neighbourhood areas will consider an integrated design process to identify energy conservation opportunities in the design.
- 30 Integration of urban agriculture opportunities is encouraged.

### Building Design

- 31 Expressing sustainability in the overall building form and details is encouraged.
- 32 Use optimal shading strategies for passive solar design that include:
  - Maximizing south facing, and minimizing north facing windows
  - Using shading devices such as roof overhangs, shrubs, trees, trellises to reduce solar gain during the summer
  - Planting deciduous trees to provide summer shading, winter light and heat penetration
  - Using solar mass to store daytime energy and reradiate it at night
  - Window placements that allow for cross ventilation
  - Outdoor spaces (especially in multiple dwelling design) that maximize southern exposure
- 33 Minimize shadowing of neighbouring properties.
- 34 Incorporate best practices for energy efficient and durable building envelopes, including optimizing insulation values to reduce heating and cooling demands.
- 35 Use durable materials and details that require minimum upkeep.
- 36 Overhangs that protect exterior wood frame walls are encouraged.
- 37 Design residential projects for accessibility.
- 38 Design of residential projects for aging in place is encouraged.

### Sustainable Energy Use

- 39 Encourage new buildings to perform more efficiently than as laid out in BC Building Code requirements.
- 40 Encourage remodelled buildings to incorporate energy efficiency into their design through building energy audits and evaluation prior to construction.
- 41 Major developments are encouraged to provide:
  - Pre feasibility energy study
  - Tying in or creating a district energy system
  - Pre installing infrastructure including piping and conduits, as recommended
- 42 Consider renewable energy systems and advanced technologies either attached to or integrated into building exteriors.
- 43 New buildings and major renovations are encouraged to have roof space allocated, and services roughed in, for hot water solar or other alternative energy systems.



### Transportation

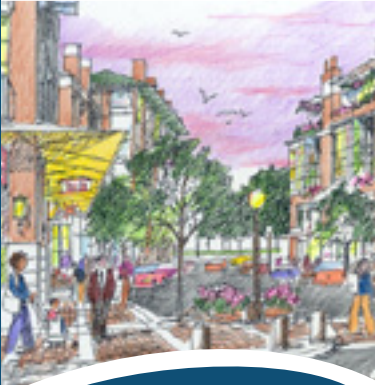
- 44 Provide standard AC power outlets for electric cars in parking areas for multiple family and mixed use buildings.
- 45 Encourage the pre installation of suitable 240 V junctions and wiring to allow the future installation of Level 2 charging stations at selected parking sites.
- 46 Provide bicycle racks and bicycle storage in multiple family, mixed use, and commercial buildings (requirements should be in zoning bylaw).



Consider prevailing wind orientation for cross ventilation and wind generated power opportunities.



Secure, convenient bicycle storage encourages use.



### **Sustainable Local Economy**

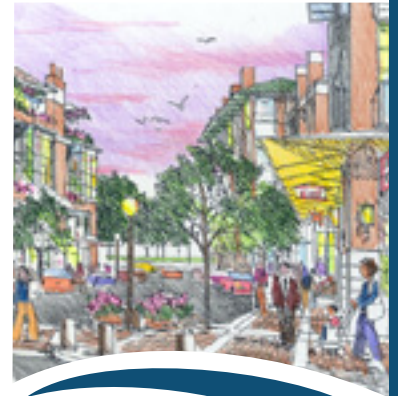
- 47 Providing spaces usable for home employment in residential buildings is encouraged.
- 48 Providing spaces which are robust and adaptable for different uses over time is encouraged.

### 3.3 Crime Prevention Through Environmental Design

Successful neighbourhood and building design reinforces the creation of safe, livable communities by providing a built environment that discourages criminal behaviour. The following strategies for providing “defensible space” in Harewood focus on commercial shop fronts, multiple family dwellings and neighbourhood design based on three CPTED strategies: Natural Access Control, Natural Surveillance, and Territorial Reinforcement. Maintenance and Management of CPTED strategies is not included in these Design Guidelines.

#### Neighbourhoods

Highly visible public spaces and traffic calming strategies increase neighbourhood safety. Neighbourhoods that attract pedestrians and have many ‘eyes on the street’ deter criminal activity.



#### Natural Access Control

- 1 Design elements that clearly indicate public spaces, including special paving treatment, landscape and gateway elements, guide visitors away from private spaces.

#### Natural Surveillance

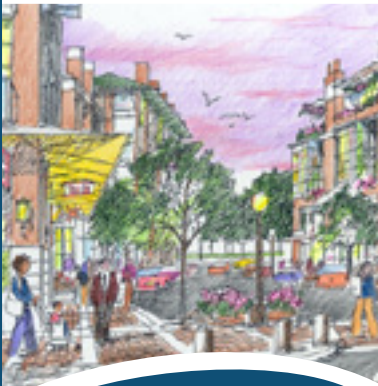
- 2 Avoid landscapes that create blind spots or hiding places.
- 3 Locate open green spaces and recreational areas to be visible from nearby homes and streets.
- 4 Laneway developments provide ‘eyes on the lane’.
- 5 Use pedestrian scale lighting in high pedestrian traffic areas to help people recognize potential threats at night.
- 6 Ensure bus stops, and the routes to them, are well illuminated at night and visible from adjacent properties.
- 7 Provide well designed public open spaces to attract users throughout the day, and that have surveillance from adjacent properties at night.
- 8 Night time lighting is essential in public open spaces.
- 9 Avoid covered outdoor areas in public spaces that may provide opportunities for loitering.



Green spaces located close to units are visually accessible, safer and better used.

#### Territorial Reinforcement

- 10 Design lots, streets and houses to encourage interaction between neighbours.
- 11 Clearly identify homes with street address numbers that are visible at night.
- 12 Define property lines with gates, low fences and shrubs.



## Commercial Shopfronts

In order for businesses to succeed, commercial streets need to be and feel safe for customers and neighbourhood residents.

### Natural Access Control

- 13 Clearly indicate public versus private spaces to discourage non employees from entering private space.
- 14 Prevent access to roofs.
- 15 Provide entrances into shops from rear parking lots.
- 16 Clearly indicate parking areas and entrances.

### Natural Surveillance

- 17 Install windows for surveillance onto adjacent parking lots.
- 18 Illuminate shop exteriors and parking lots at night.
- 19 Avoid hiding areas around loading bays.
- 20 Maintain visibility between passing vehicles, parking areas, sidewalks and shop interiors.
- 21 All entrances should be visually accessible.

### Territorial Reinforcement

- 22 Mark property boundaries wherever possible with low fences, gates or low hedges.
- 23 Clearly distinguish private from public property.
- 24 Include shop signs facing rear parking lots.
- 25 Include awnings above windows and doors.
- 26 Provide landscape and ensure a tidy appearance at the rear of shops.



Parking lots are safer when there is visual access from an adjacent building.

## Multiple Dwellings

Multiple Dwellings can capitalize on ‘safety in numbers.’ Well connected communities are often the safest ones by providing pedestrian activity.

### Natural Access Control

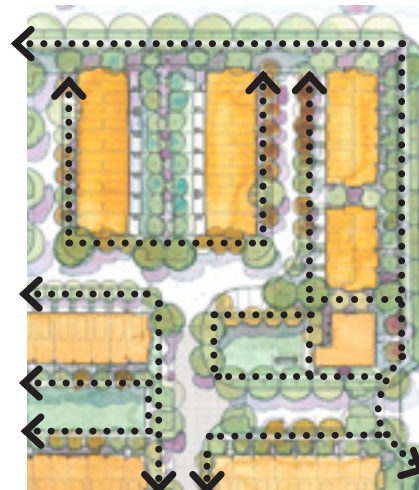
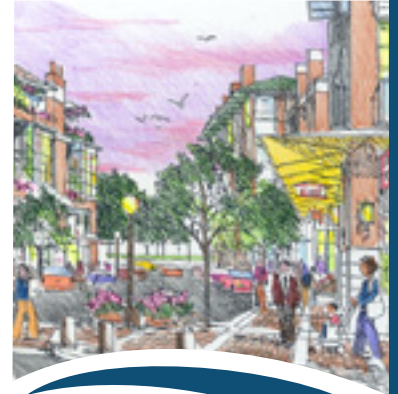
- 27 Clearly define site entrances, including parking areas.
- 28 Provide good illumination in public spaces, especially hallways.
- 29 Eliminate dead end and visually inaccessible interior and exterior areas.
- 30 Design visual access to elevator lobbies and stairwells.
- 31 Limit the number of building access points to two.
- 32 Common building entrances should automatically lock upon closing.

### Natural Surveillance

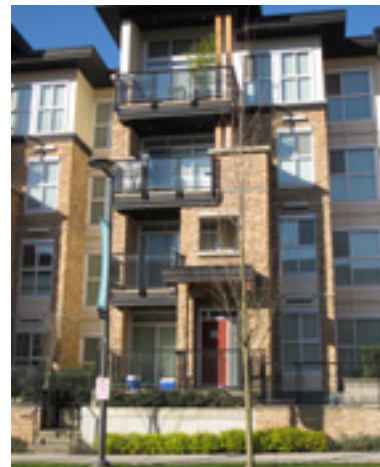
- 33 Design exterior doors that are visible to the street and neighbours.
- 34 Install windows on all building sides.
- 35 Discourage unauthorized parking by assigning specific parking stalls to residents that are near their units.
- 36 Designate visitor parking.
- 37 Illuminate parking areas and pedestrian walkways.
- 38 Locate recreation areas so they are visible from residential units.
- 39 Elevators and stairwells should be clearly visible.
- 40 Restrict landscape height to 1.0 metre in vulnerable areas.
- 41 Site buildings to allow visibility between units without direct overlook.
- 42 Playgrounds should be visible from units but not located adjacent to parking lots or streets.

### Territorial Reinforcement

- 43 Define property lines with landscaping or fencing.
- 44 Maintain visibility between units and the street by restricting the height of landscape, screens and fences.
- 45 Accentuate building entrances with architectural elements, lighting and / or landscape.
- 46 Individual mailboxes at unit entrances are more secure than group mailboxes.



Incorporating a network of pedestrian routes in multiple dwelling developments increases safety and provides connection to the surrounding neighbourhood.

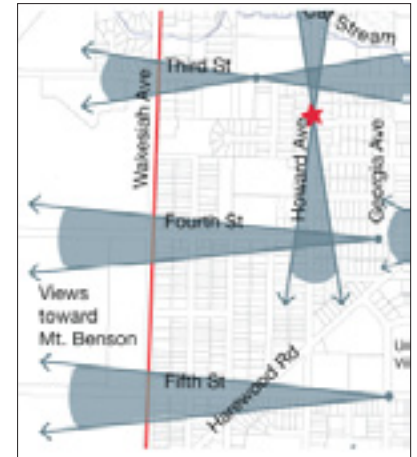
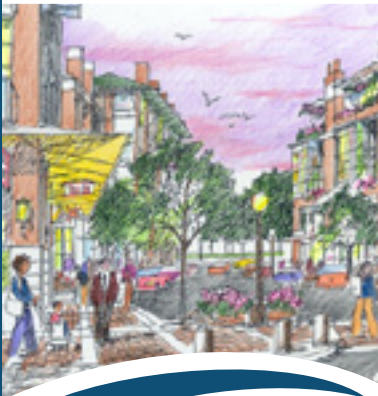


Multiple dwelling developments provide many ‘eyes on the street’.

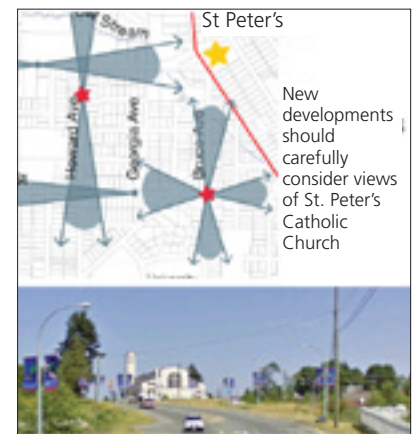
### 3.4 View Corridors

Harewood boasts a number of significant view corridors. The form and massing of new developments should carefully consider their impact on this important aspect of the neighbourhood character.

- 1 Public street view corridors, as shown on Figure 7, should be maintained.
- 2 Mount Benson is a dominant backdrop for the neighbourhood. The impact of new development on panorama views of Mount Benson should be carefully considered.
- 3 There are a limited number of street end views to the waterfront in Harewood. These views should be retained wherever possible. Potential street end views that emerge as the neighbourhood develops, including revised street rights of way and front setbacks, should be integrated into new development.
- 4 Views of neighbourhood landmarks, including St. Peter's Catholic Church, are important aspects of the neighbourhood character and should be respected by new neighbourhood development.
- 5 View impact analysis for sites affected by a view corridor, as noted in Figure 7, shall be undertaken at the development approvals stage to establish appropriate building locations and heights. Development applicants will be required to undertake analysis of view impacts from upland sites to be selected in consultation with City staff.



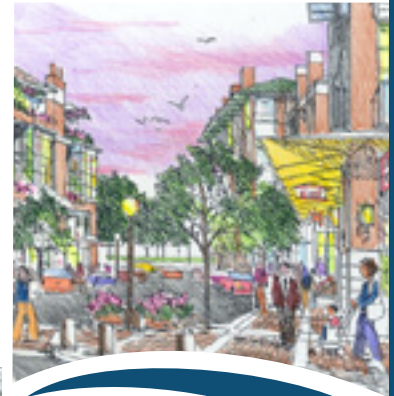
Panorama views of Mt. Benson contribute to the neighbourhood character and should be retained.





### 3.5 Roof Forms

- 1 Roof design should incorporate the following principles:
  - Clear, simple primary roof forms
  - Overhangs consistent with passive solar strategies
  - Low pitches with large overhangs allow rain protection without blocking low sun angles in winter
  - Roof forms should relate to the overall massing and contribute to an attractive building profile
  - Refer to Section 6.1 Roofs for guidelines respecting townhouse roof forms



Varied roof forms and / or heights should relate to elements in the overall building form and massing.



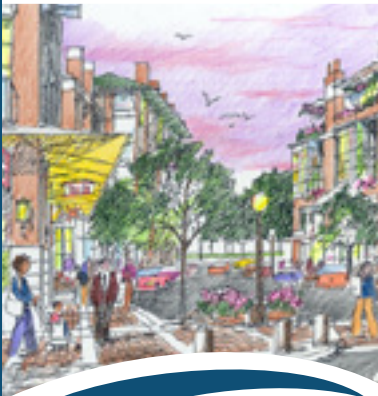
Roof overhangs and / or screening devices should provide weather and sun protection.



Deep overhangs provide shading and weather protection.



Developments with flat roofs and facades depend on good proportions, high quality materials and a well articulated entrance.



### 3.6 Off Street Parking and Loading

#### General Off Street Parking and Loading

- 1 Minimize the footprint of parking areas through measures such as shared parking areas, reduced parking requirements near transit routes, and providing reserved parking spaces for car share programs.
- 2 Parking areas that are shared between multiple units and / or provide parking based on complementary parking demands such as commercial day time parking and residential night time parking are highly encouraged.
- 3 Provide access via rear mid block lanes, and minor streets when necessary.
- 4 Minimize access from major thoroughfares to reduce the impact on through traffic.
- 5 Providing charging stations for electric cars is encouraged.



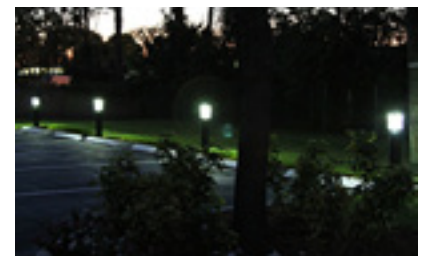
Reduce the effects of parked cars on adjacent properties, streets or lanes with landscape and screens.

#### Surface Parking For Developments Up To 3 Storeys

- 6 Generally, off street parking should be separated and screened from the streets.
- 7 Site surface parking should be accessed from the lane, where there is a lane. If there is no rear lane, the parking should be accessed by a side driveway from the fronting street.
- 8 For corner lots, parking should be accessed from the flanking street, if there is no lane.
- 9 Landscaped areas and dedicated pedestrian circulation pathways around and through parking lots, are encouraged to break up the size and visual impact of larger parking lots.
- 10 Loading areas shall be at the rear of buildings, accessed from the lane where possible, and screened to reduce visual impact.
- 11 Garbage and recycling should be contained and screened from view.
- 12 Parking areas should have adequate levels of lighting for safety and visibility.



Permeable surfaces and landscaped areas or rain gardens are recommended rainwater management strategies.



Lighting is a basic requirement in parking lot design.



Well designed screens for garbage and recycling improves the lanescape.

### Underground / Under Building Parking For Developments 4+ Storeys

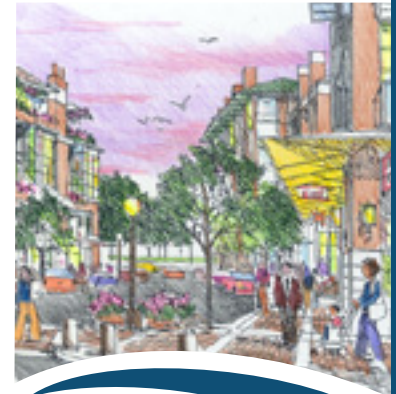
- 13 Underground and under building parking access should be from the lane, or if no lane, from the flanking street for corner sites.
- 14 Landscapes, trellises or other design elements are recommended to enhance the appearance of parkade entrances.
- 15 Materials and signage should be attractive, enhancing the street or lane providing access.
- 16 Minimize the width of drive aisles to reduce the visual impact of the parking entrance.
- 17 The visual impact of parkade gates and interiors from the street should be reduced by careful design of lighting and by use of appropriate materials and colours.



Landscape improves the appearance of underground parking access.



Underground access should be integrated into the overall architecture.

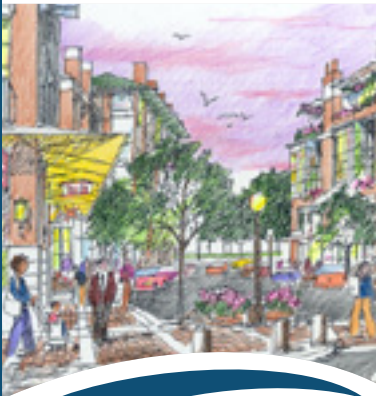


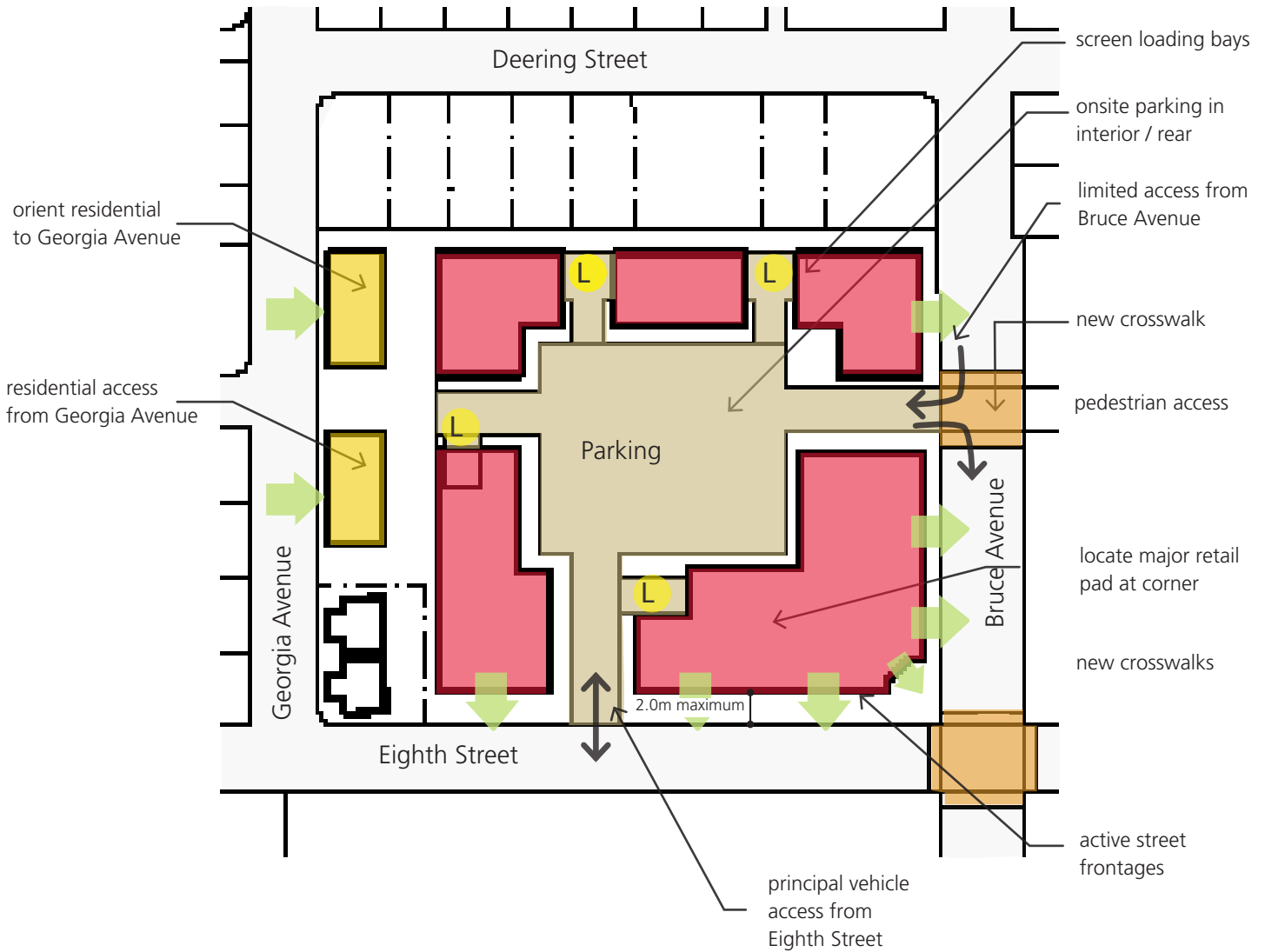
## 4 Building Design Guidelines South Harewood Village

The intent of these guidelines is to encourage the development of a neighbourhood commercial centre that provides goods and services to the South Harewood neighbourhood, that is carefully planned as an urban centre, that is compatible in form and scale with the surrounding development, and that is designed to optimize pedestrian usage and amenity.

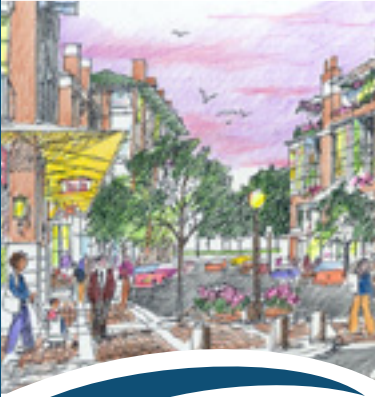
### 4.1 General Guidelines

- 1 This neighbourhood commercial centre is defined as the block bounded by Georgia Avenue, Eighth Street and Bruce Avenue, south of Deering Street, excluding the townhouse development at the corner of Georgia Avenue and Eighth Street.
- 2 Development of this large site should aim to create a small scale urban, pedestrian oriented retail and service node that surrounding residents can access and use safely and easily, and less of a typical suburban auto oriented mall. The site planning should orient the bulk of new commercial buildings towards the surrounding streets, and locate any on site parking in the interior or rear of the site. Vehicle access should be carefully managed to provide the principal vehicle access from Eighth Street and more limited access from Bruce Avenue.
- 3 Commercial buildings should be broken down in scale and size into a number of smaller buildings, to reduce the visual and physical impacts on the surrounding residential area. Buildings should be oriented and designed to present active frontages to the surrounding streets as much as possible.





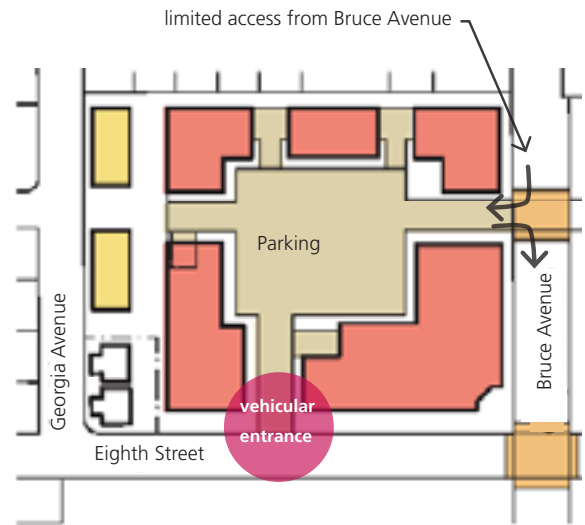
NOTE: This conceptual diagram is intended to illustrate the following design guidelines. It is not intended to be a definitive design for the site, but does illustrate many of the principles and should be referred to when preparing or considering a development application for this site.



## 4.2 Site Planning Guidelines

### Access and Circulation

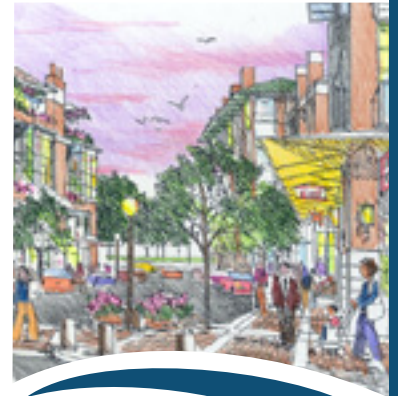
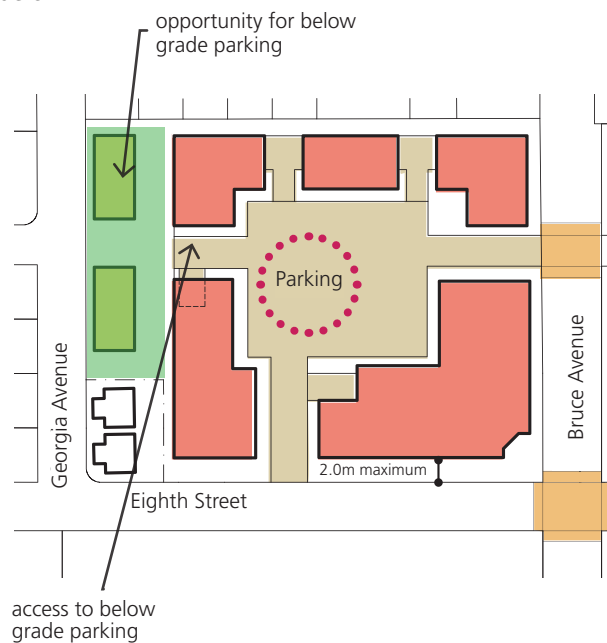
- 4 Primary vehicular access should be from Eighth Street, located approximately midblock between Georgia Avenue and Bruce Avenue.
- 5 A secondary limited vehicular access point may be considered from Bruce Avenue, provided this access does not interfere with the redevelopment of Bruce Avenue as a major collector with a central median. This access may need to be limited to right-in, right-out access movements only. This secondary access point should be located midblock, opposite the extension of the Murray Street right of way.
- 6 No vehicular access is to be permitted from Georgia Avenue.
- 7 Pedestrian access is to be provided from all surrounding street fronts, with continuous, generous sidewalks. Crosswalks should be provided or enhanced in all four directions at the intersection of Bruce Avenue and Eighth Street. A midblock pedestrian crosswalk should be considered across Bruce Avenue, opposite the existing pedestrian pathway that connects Bruce Avenue and Murray Street.



### Parking

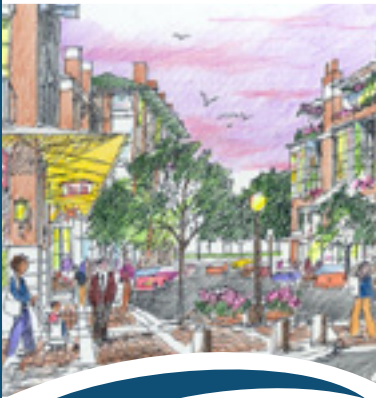
- 8 All on site parking should be located in the interior or rear of the site, behind the commercial buildings. No on site parking should be allowed between the buildings and the surrounding streets.
- 9 Surface parking should be carefully designed to break up large paved areas through the use of curbs, medians, pedestrian pathways, trees, landscape, fences, hedges, water features, and other elements. On site parking areas should be designed in small parking islands, and be safe for pedestrians, with adequate lighting, clearly marked walkways, crosswalks and refuge areas.

- 10 Given the significant elevation difference between Georgia Avenue (elevation +41.8 metres) and the bulk of the site (elevation +34.5 metres), there is an opportunity to bury some on site parking within the topographic slope on the western portion of the site, in a structure beneath the existing grade. Any such structured parking should be accessed from within the site, and not from Georgia Avenue. Such structured parking could also serve any residential uses that may be developed on the western portion of the site facing Georgia Avenue, as noted below.



## Building Locations

- 11 Commercial buildings should be sited toward the perimeter of the site, with the centre of the site left open for any on site parking. The largest retail pad should be located prominently nearest the intersection of Bruce Avenue and Eighth Street. Smaller retail pads may be located further into the site, towards the rear, fronting onto the central parking area.
- 12 Retail spaces should be set back a maximum of 2.0 metres from the property line to provide space for merchandise display and / or cafe seating.
- 13 Any residential buildings should be located in the western portion of the site and oriented towards Georgia Avenue, which is a residential street.



### 4.3 Building Guidelines

#### Frontages and Orientation

- 14 Retail buildings should be oriented toward the adjacent streets. Stores should ideally be accessed directly from the adjacent street, with secondary entries facing the parking area towards the interior of the site.
- 15 Storefront facades along streets shall be active and animated. No continuous blank facades shall be permitted adjacent to any street.
- 16 Buildings should reinforce the streets as the primary public space, with facades that parallel the street and help define and frame the adjacent sidewalk.



#### Building Form and Character

- 17 Commercial buildings should be designed to be compatible in form, scale, and materials with the surrounding emerging urban context. The maximum size for any single commercial retail store is 3700 m<sup>2</sup> (39,800 ft<sup>2</sup>). Buildings should be restricted to a maximum length of 24 metres. Long facades should have architectural detailing to break up the length and provide visual interest, such as pilasters, bays, recesses, individual windows and doorways, setbacks, and so on.
- 18 Commercial buildings should have active facades that provide transparency between the adjacent sidewalks and the store interiors. Blank facades are not permitted facing adjacent streets.
- 19 Building form should clearly express its purpose and use. The use of pedestrian scaled architectural elements is encouraged, such as

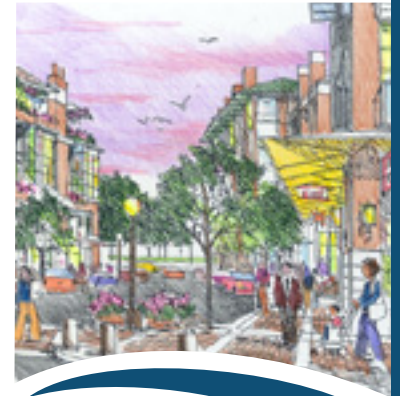


individual shopfront bays, entry recesses, courtyards, roof overhangs, weather protection awnings and canopies, large windows, and expressive colours.

- 20 Commercial signage should be limited, discreet, and designed to blend with the building, not dominate it.
- 21 Refer to Section 5.1 Commercial Shop Fronts for detailed shop front guidelines.

#### 4.4 Landscape Guidelines

- 22 Hard and soft landscape shall be used extensively to add texture to the overall development, provide visual relief, and screen loading bays, garbage and recycling areas, and parking entrances.
- 23 Continuous street trees shall be planted along all surrounding streets, and on any internal roadways.
- 24 Landscaping features are encouraged at all entries into the site.
- 25 Sidewalks shall be designed to City standards and current best practises shall be adhered to for street trees, sidewalk paving, drainage, street furniture, signage, pedestrian lighting, and so on.
- 26 Seasonal landscape features such as hanging flower baskets, planter beds, planted medians, and specimen trees / plants, are encouraged.
- 27 Landscapes that help create a more sustainable urban environment are encouraged. This could include rainwater detention ponds, drainage areas and swales, pervious surfaces in parking areas, drought resistant and low water use species, native species, and large canopy deciduous trees that convert CO<sub>2</sub> to oxygen.



Commercial setbacks support places for seating and retail display.

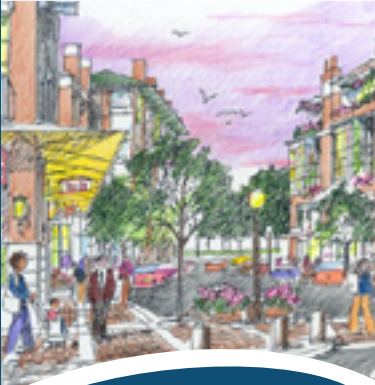


Landscapes soften the appearance of parking areas and are an opportunity for rainwater management.



## 5 Building Design Guidelines

### Mixed Use Development Up to 6 Storeys

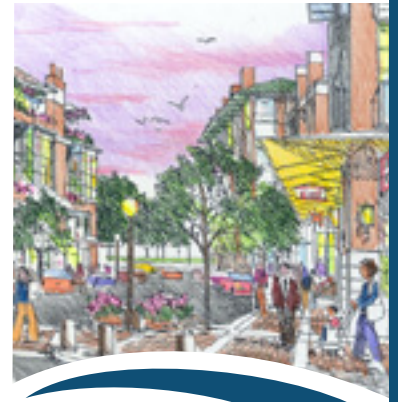


Mixing residential with commercial uses within a development benefits streets and neighbourhoods. Residential use provides 'eyes on the street' and extends activity on the street throughout the day. Mixed Use developments have the potential to increase housing variety and density, reduce distances between housing and workplaces, and support pedestrian and bicycle friendly environments.

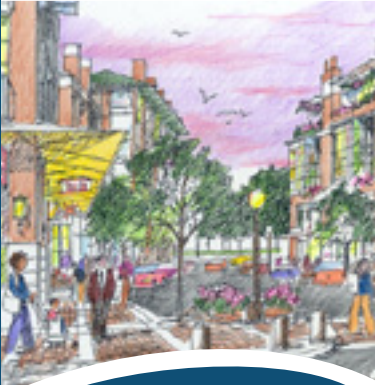
#### 5.1 General Guidelines

- 1 Roof forms have a large impact on the streetscape. These Guidelines recommend simple roof forms to encourage compatible neighbours.
- 2 Intersections are place making opportunities. Corner buildings that address the main and flanking streets are encouraged. Distinctive architectural forms and elements at the end of blocks are encouraged.
- 3 Breaks in the streetwall can provide access to lane oriented parking, add variety to the street and create opportunities for unique places including courtyards or pocket parks / plazas.
- 4 Well articulated street facades including generous window sizes, bay windows, balconies and window boxes are encouraged to animate the street.
- 5 Shopfronts that include pedestrian oriented signs, space for seating, retail display or planting contribute to the street character and indicate a well cared for place.
- 6 Residential entrances along mixed use streets should fit into the streetscape and be clearly identified.

*Please note: The adjacent images are conceptual form and character illustrations of Harewood Village, identified in the Urban Design Framework, Section 1.1 Corridor Development.*



MIXED USE UP TO 6 STOREYS  
URBAN DESIGN GUIDELINES



## Form and Character

### General

- 1 Provide visual breaks for buildings longer than 45 metres.
- 2 Provide weather protection, landscaping and architectural detail at the street level to support walkability.
- 3 Consider the use of balconies, awnings, signage and architectural features to create articulation, texture, rhythm and visual interest on building elevations.
- 4 Simple combinations of materials and forms are recommended.
- 5 Architectural elements and details should be consistent with the overall building design.
- 6 Natural building materials such as wood, stone, brick and metal to be used for construction.
- 7 The use of high quality, durable materials are expected.



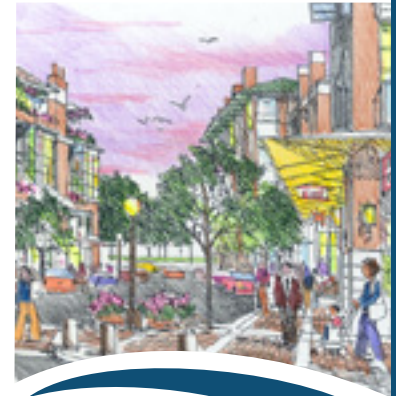
Changes in plane and variation of colours and materials reduce the overall building scale.



The open and transparent ground floor commercial spaces provide a distinct building base, differentiating commercial from residential uses.

**Side and Rear Elevations**

- 8 Front elevation design features to be utilized on side and rear elevations. Where side walls are without openings, textured materials, details or simple decorative accents are encouraged, and should relate to the primary architectural features of the building.
- 9 Side and rear elevations should be attractive to adjacent neighbours.
- 10 Rear entrances should incorporate rain protection, appropriate signage and exterior lighting.



**Balconies**

- 11 Balcony design should provide for covered, usable space for residents and support the overall design intent.
- 12 Soffit materials should be resistant to weathering and provide an attractive ceiling for neighbours below.
- 13 Guardrail design should be architecturally consistent with the overall design intent.



Coloured or textured materials are encouraged for blank walls.



Rear elevations and parking lots should be attractive to neighbours who overlook them.

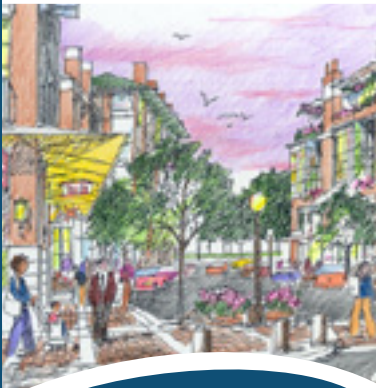


Materials, details and the form of balconies should be consistent with the overall building design.

## Commercial Shop Fronts

### Materials and Proportions

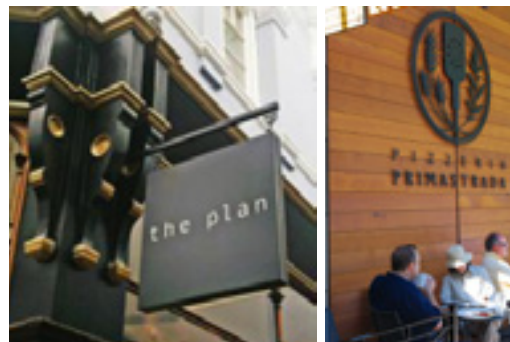
- 14 A variety of shops and services are important to successful mixed use developments.
- 15 Narrow frontage shops are preferred to large format retail to increase variety and pedestrian interest.
- 16 The form and character of businesses with commercial shop fronts, including large format retail, should be carefully considered. Incorporating small street fronting shops to reduce the impact of larger commercial uses is recommended.
- 17 A 4.0 metre minimum dimension for ground level floor to floor heights is recommended.
- 18 A 4.0 metre maximum rhythm of shop front bays is recommended. Visual breaks are required at maximum 4.0 metres on commercial frontages, to encourage vertical shop front bay proportions.
- 19 Generous areas of shop front glazing provide views into and out of shop interiors.
- 20 Panelling or tiles at the base of shop fronts are recommended.
- 21 Ground floors should provide a well defined building base.
- 22 Shop front windows should comprise no less than 75% of the total shop front area.
- 23 Shop front exteriors should be constructed of high quality materials that are durable, resistant to graffiti, complement the overall appearance of the building, and are compatible with adjacent shop fronts.
- 24 Natural building materials such as wood, stone, brick and metal are encouraged for construction.
- 25 The use of vinyl or aluminum siding, or other materials made to imitate natural finishes, is discouraged.



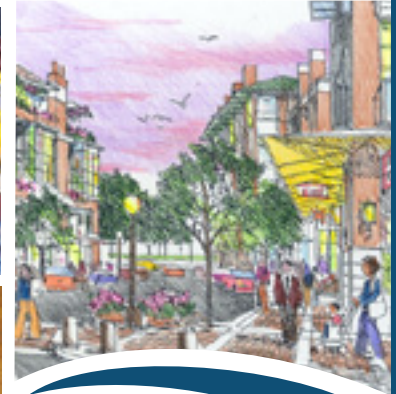
A number of strategies have been used to provide variation along this streetscape of two business frontages, including changes in material and plane, and variation in the design of weather protection, entrances and glazing patterns.

**Signage**

- 26 Signs that are oriented to pedestrians are recommended including: overhead hanging signs perpendicular to the shop front, painted window signs, signage incorporated into awnings or canopies, and other methods of reinforcing the identity and character of the business.
- 27 Signs should complement the style, composition, materials, colours and details of the building.
- 28 Overhead signs should not extend above the first storey or conflict with design details, windows or cornices.
- 29 Window signage should be limited to 15% of available window space.
- 30 Backlit acrylic signs are discouraged, but may be considered where they are incorporated into an architectural feature.



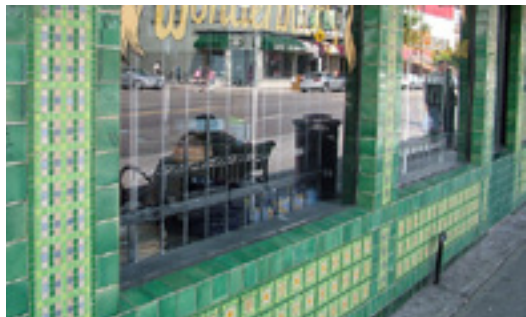
Well designed commercial signs complement the overall building design and contributing to a vibrant streetscape



Entry doors, signage or other shop front design elements should address corner locations.



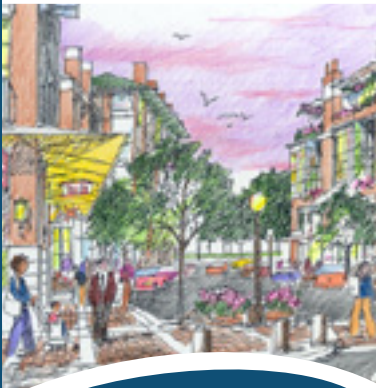
High quality materials, transparent shop fronts and unique signage and variety support walkability.



Ceramic tiles are recommended for their range of colour, style and durability.



Shop fronts that combine variety and transparency are preferred.



### Entrances

- 31 Shop front entrances should be clearly marked and provide a visual focus for the commercial exterior.
- 32 Entry doors recessed into the shop front are recommended to increase variation along the streetwall.
- 33 Shop entrances set into the building face maximize opportunities for glazing and signage, and reflect local heritage elements.
- 34 Shop fronts that wrap around street corners are encouraged, marking corners as activity nodes and providing variety along the street.



Recessed shop doors extend the window area for retail display and accentuate the shop entrance.



Corner buildings should address the principal and flanking streets.

### Awnings and Canopies

- 35 Continuous weather protection is encouraged along streets.
- 36 Awnings and canopies provide opportunities for signage, colour and weather protection.
- 37 High quality materials including canvas and fire resistant acrylic are preferred awning materials.
- 38 Awnings on a multiple storefront building should be consistent in character, scale and location, but need not be identical.
- 39 Canopies or overhangs should emphasize individual shop fronts, thereby helping to break down large building masses and reducing the impact of larger development forms.
- 40 Innovative methods of directing rainwater from awnings and canopies into street level rain gardens is encouraged.



The design of awnings and canopies impact the quality and character of the street.

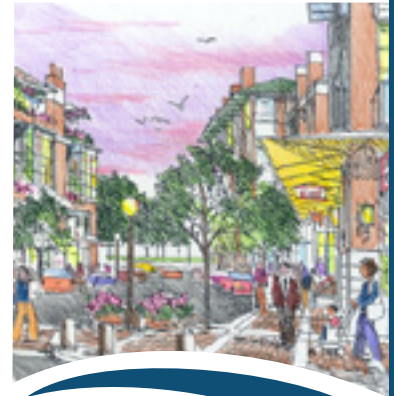


**Landscape and Street Furniture**

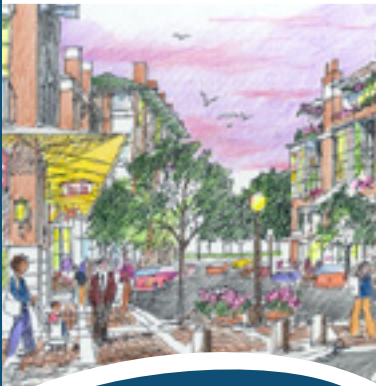
- 41 Planters, window boxes and / or seating integrated into commercial exteriors are pedestrian friendly and help animate the streetscape.
- 42 Materials and details should be durable and complement the overall shop front design and add to the character of the street.

**Exterior Lighting**

- 43 Exterior lighting on mixed use streets should illuminate facades, entrances and signage, and provide adequate light levels for personal safety.
- 44 Exterior lights should not produce glare onto residential streets, public ways, adjacent properties, or the sky.
- 45 Lighting fixtures should complement or highlight the building facade design.
- 46 Energy efficient lighting should be utilized, including timers or motion detectors, to minimize energy consumption.



Well designed exterior lighting benefits the streetscape and increases pedestrian safety.



## Residential

The following guidelines apply to residential entrances on the first (ground) floor in mixed use developments.

### Materials and Proportions

- 47 Residential elements in mixed use buildings should be distinguishable from and complementary to adjacent commercial uses.
- 48 High quality materials that complement the overall building design and contribute to the streetscape are expected.

### Entrances and Signage

- 49 Residential entrances in mixed use buildings should complement adjacent shop front proportions but be clearly identified as residential.
- 50 Recessing entrances into the building facade provide weather protection and privacy.
- 51 Incorporating planting and seating into the building entrance contributes to a residential identity and variety along the street.
- 52 Residential signage, including addresses, should be clear, obvious and consistent in quality with adjacent shop fronts.
- 53 On corner sites, residential entries should be on the flanking street.

### Exterior Lighting

- 54 Exterior lighting should clearly illuminate the residential entrances, address and exterior lobby.
- 55 Exterior lights should not produce glare onto streets, public ways, adjacent properties, or the sky.
- 56 Lighting fixtures should complement the building facade and be distinguishable from adjacent commercial lighting.
- 57 Energy efficient lighting should be utilized, including timers or motion detectors, to minimize energy consumption.



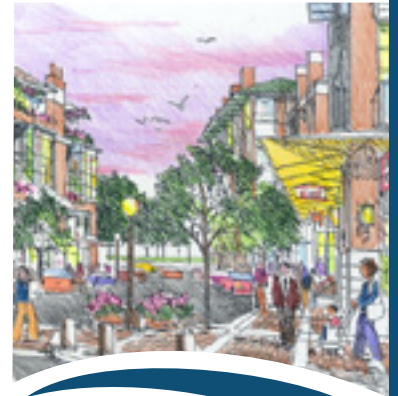
The design of common entrances should be clearly identified and contribute to the streetscape.

## 5.2 Mixed Use Massing Typologies

### Up to 3 Storeys

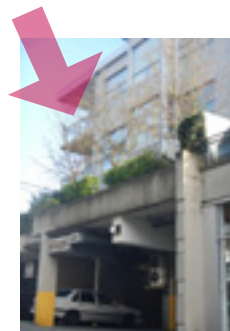
#### Commercial / Residential Mix

- 1 Single storey commercial developments are not permitted
- 2 Two storey developments are required to locate:
  - Commercial / retail space on the first (ground) floor and
  - Residential use on the second floor
- 3 Three storey developments are required to locate:
  - Commercial / retail on the first (ground) floor and
  - Either office or residential space on the second floor and
  - Residential use on the third floor

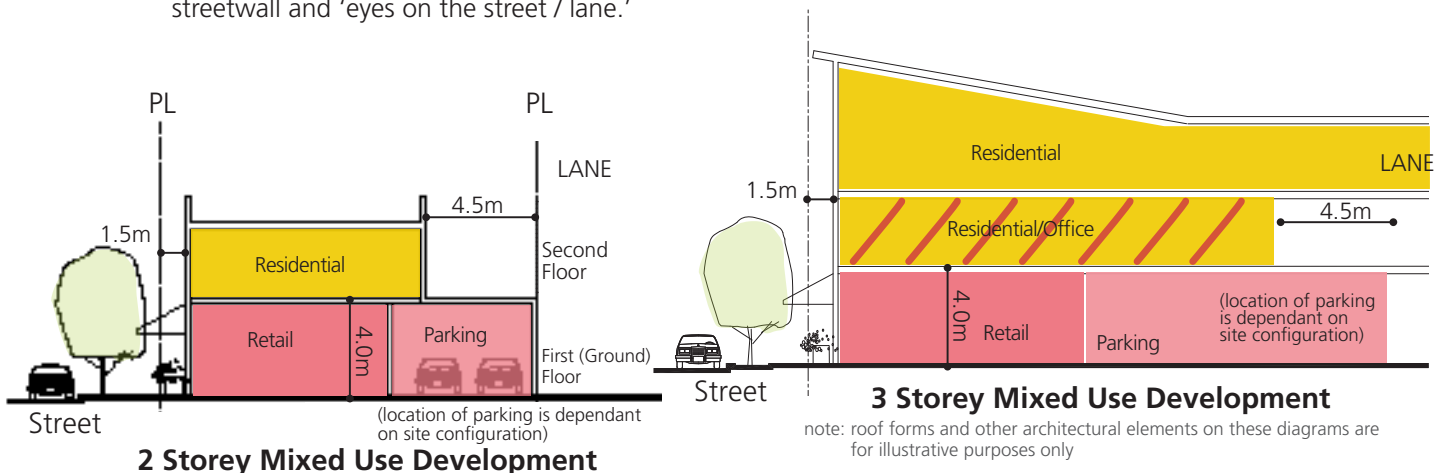


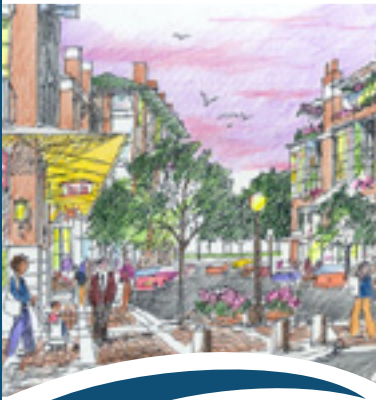
#### Massing and Setbacks

- 4 Mixed use commercial developments to be set back 1.5 metres from the front or major flanking street property line to allow for retail display space, landscape and / or seating, except in the Neighbourhood Commercial Village Centre.
- 5 Residential and / or office space to be set back 7.5 metres from the centre line of the lane to provide access to light and privacy. Where there is no lane, residential and / or office space to be set back 7.5 metres from the rear property line. No setback is required from interior side yards.
- 6 Ground floor commercial uses are not required to be set back from rear or interior side property lines but additional setbacks may be required to accommodate servicing, parking or landscaping.
- 7 Balconies are encouraged to provide activity along the streetwall and 'eyes on the street / lane.'



Landscape and / or deck spaces are encouraged where the building is set back 4.5 metres from the rear property line.

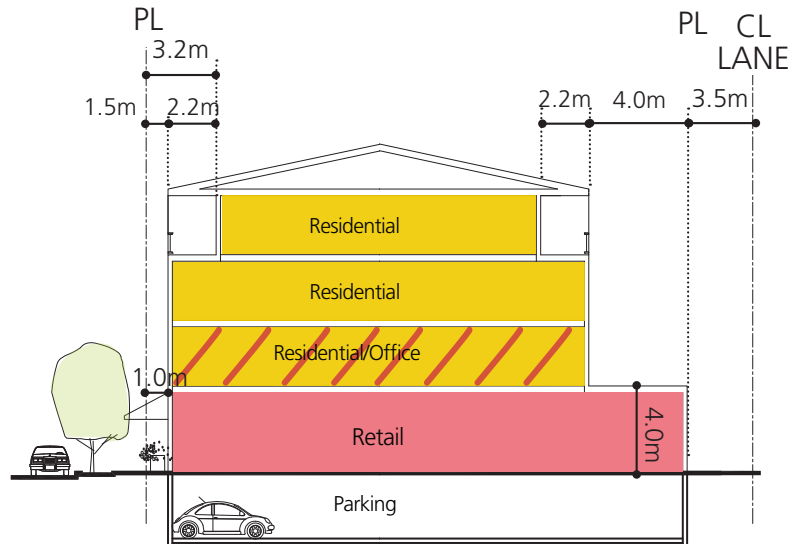




## Up to 6 Storeys

### Commercial / Residential Mix

- 1 Retail / Commercial in select locations is required on the first (ground) floor (see Map 1 - Neighbourhood Land Use).
- 2 Residential and / or office space is allowed on the second floor where more than two storeys are constructed.
- 3 The third to sixth floors are required to be residential.

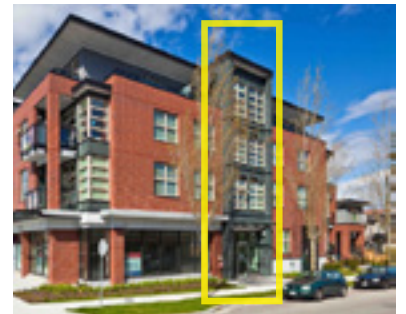


### 4 Storey Mixed Use Development

note: roof forms, balconies and other architectural elements on this diagram are for illustrative purposes only.

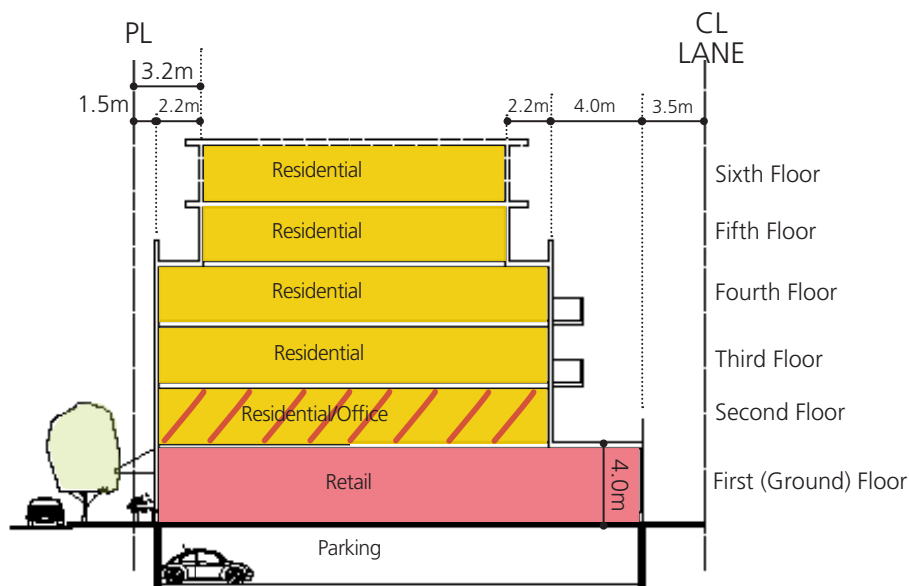
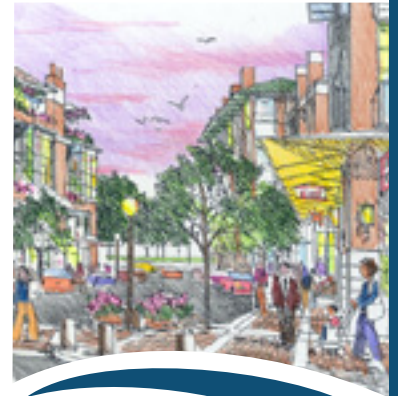
### Massing and Setbacks

- 4 All mixed use developments to be set back 1.5 metres from the front or flanking street property line to allow for retail display and / or seating.
- 5 Four storey developments should set back the fourth floor an additional 2.2 metres from the front property line and from the rear setback line.
- 6 Five and six storey developments to set back the fifth and sixth storeys an additional 2.2 metres from the front property line and 2.2 metres from the rear and the face of the wall below.
- 7 Residential and / or office space to be set back 7.5 metres from the centre line of the lane to provide access to light and privacy. Where there is no lane, residential and / or office space to be set back 7.5 metres from the rear property line.



The building entrance and stairwell extends beyond upper floor setback.

- 8 Ground floor commercial uses are not required to be set back from rear or interior side property lines.
- 9 Portions of four, five or six storey buildings adjacent to neighbouring developments, at street corners, or to emphasize building elements such as stairwells or entrances, can maintain the 1.0 metre setback from the front property line.



### 5 and 6 Storey Mixed Use Development

note: roof forms, balconies and other architectural elements on this diagram are for illustrative purposes only.

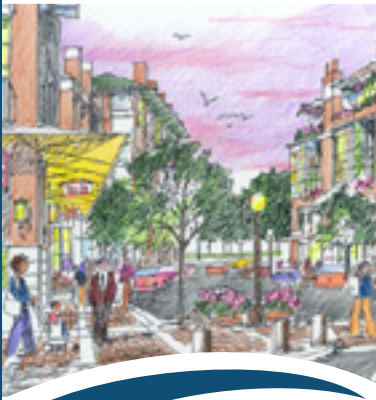
### Transition to Lower Density Neighbours

- 10 Setting back residential storeys above the ground floor buffers units from the lane, minimizes overlook and shadowing onto adjacent residences, and provides private outdoor spaces.
- 11 Landscape incorporated into upper floor setbacks minimizes overlook into adjacent property, softens the building edge, and creates visual interest above the street level.



Stepping building forms, from the street to the lane, help ease the transition from higher to lower density developments.



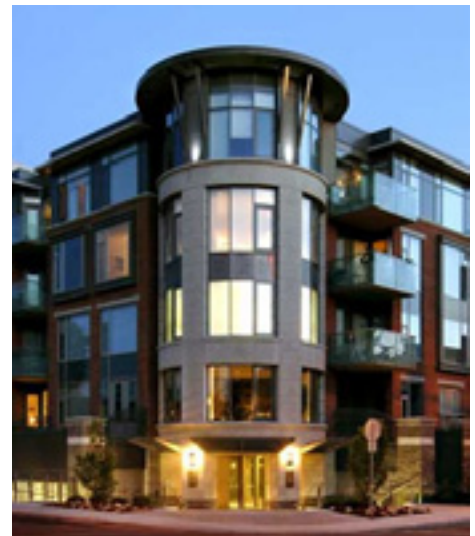


### Conditions at Prominent Street Corners

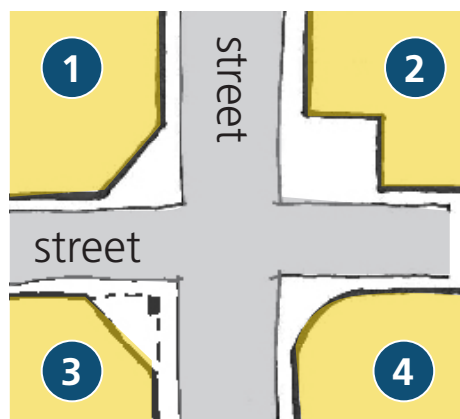
Opportunities for architectural expression and enhanced public spaces occur at prominent street corners such as the intersections of Fifth Street at Wakesiah Avenue or Bruce Avenue.

- 1 A significant portion of the building should address the corner intersection.
- 2 Examples of corner treatments are illustrated here:

- 1 Chamfered building corners increases the amount of public space and addresses both streets.
- 2 Small corner plazas can provide space for seating or enhance shop entrances.
- 3 Curved elements can create a unique building form, adding variety to the streetscape.
- 4 Setting back only the ground floor creates a sheltered public space.



An example of a curved corner element.



Well designed corner plazas are good gathering places.

## 6 Building Design Guidelines

### Residential Development Up To 6 Storeys

#### 6.1 Townhouses Up To 3 Storeys

##### Form and Character

###### Entrances

- 1 The design of townhouse entrances should continue existing neighbourhood patterns, including front gates, gardens, lighting and address signs.
- 2 Front entrances should be 2 to 4 risers above the adjacent sidewalk level.
- 3 Front doors and entries should be visually prominent and clearly associated with the dwelling unit.



###### Balconies

- 4 Balconies are recommended to provide additional private outdoor space.
- 5 Balcony locations should respect neighbour's privacy.
- 6 Access to sun increases the usefulness and enjoyment of balconies.
- 7 Materials and details, including guardrails, should be consistent with the design intent of the townhouse.



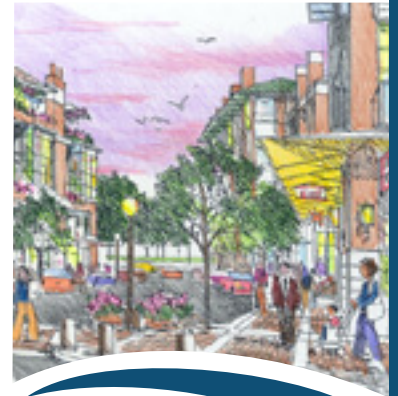
Each dwelling is distinct and has a clearly identified entrance.

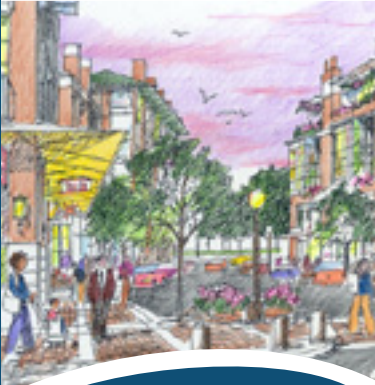
###### Roofs

- 8 Roof forms supporting a distinct identity for individual townhouse units are encouraged.
- 9 Gable roof pitches between 4:12 and 8:12 are preferred.
- 10 Simple roof designs are encouraged.
- 11 Refer to Section 3.5 Roof Forms, for additional guidelines for roof design.



Side elevations should contribute to the streetscape.





### Side and Rear Elevations

- 12 Side and rear elevations should be attractive and contribute to the overall design of the townhouse development.
- 13 High quality materials used on the front elevation should be continued along side elevations to avoid 'wallpaper' effect, where materials appear pasted onto front elevation.



Rear elevations should be attractive and consistent with the quality of the overall design.

### Signs and Addresses

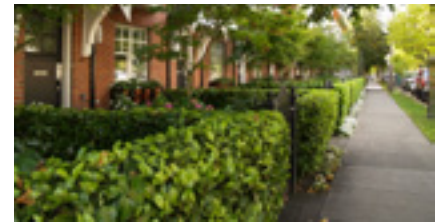
- 14 Each townhouse unit should have a clear, well lit address, adding to the identity of each unit.
- 15 Integrating addresses and lighting into the unit entry or garden gate design is recommended.

### Exterior Lighting

- 16 Design lighting that complements the overall building design is recommended.
- 17 Integrating lighting into pathways from the front sidewalk and / or lane is recommended.
- 18 Light spillage and glare is to be avoided.

### Landscape

- 19 Landscape design should complement the entire townhouse development.
- 20 Soft and hard landscapes should extend the livability of townhouse developments and provide year round colour and texture.
- 21 Native plant species are recommended.
- 22 Avoid landscapes that can create blind spots or hiding places.
- 23 Rainwater management should be carefully integrated into the landscape design of all private and common spaces, including the use of permeable surfaces, swales and rain gardens.
- 24 The collection, storage and infiltration of rainwater should be an important design consideration.



High quality private outdoor space enhances townhouse livability.



**Private Outdoor Space**

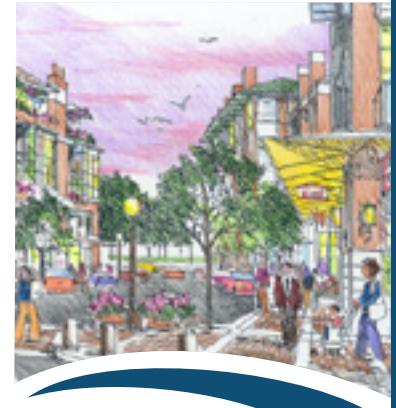
- 25 Each townhouse unit should have a minimum of 10 m<sup>2</sup> of private outdoor space on grade or at the rooftop level.
- 26 Access to sun and privacy are expected for residents and neighbours.

**Common Outdoor Space**

- 27 Common outdoor space is important for encouraging a sense of community.
- 28 Children’s play spaces and opportunities for recreation are encouraged.
- 29 Common outdoor spaces should have access to daylight from 10 am to 2 pm.
- 30 A balance of hard and soft landscaping is recommended for all weather use.
- 31 Common outdoor space should be visible from dwelling units to ensure surveillance by residents.



Common outdoor spaces should be accessible and useable year round.



**Garden Walls, Fences and Gates**

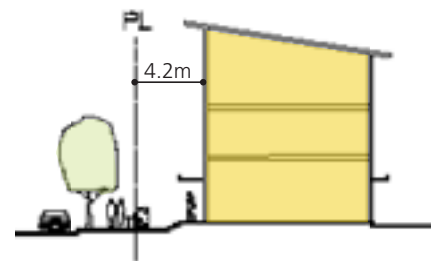
- 32 Integrating garden walls, fences and gates into higher density developments continues patterns associated with traditional single family housing.
- 33 Landscape elements to provide a clear transition from public to private space.
- 34 Details and materials should be consistent with the overall architectural approach.
- 35 Lighting and addresses should be integrated into the design.



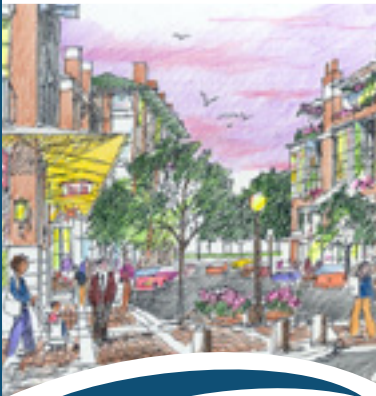
Well designed lighting, landscapes, garden walls, gates and fences create attractive and safe neighbourhoods.

**Massing and Setbacks**

- 36 Townhouses are recommended to set back 4.0 metres from the front property line.
- 37 Front entrances, including raised stoops and weather protection, are allowed to extend 2.4 metres into the front setback.
- 38 Landscape to support active use of private areas within the front setback is encouraged.
- 39 Parking should be accessed from the rear, ideally in garages or carports.
- 40 Cars should not be parked adjacent to occupied rooms.



**3 Storey Townhouse Development**  
*note: roof forms, balconies and other architectural elements are for illustrative purposes only*



## 6.2 Multiple Dwelling Units Up to 6 Storeys

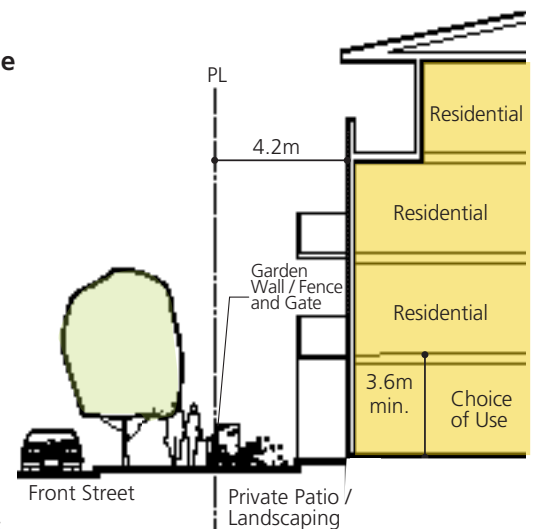
Multiple dwelling units increase housing choice for Harewood residents.

### Ground Oriented Units

Orienting ground level units in multiple dwelling developments (apartment or condominium developments) to the street integrates these higher density developments into the patterns of single family homes and townhouses. Two storey ground oriented units are highly encouraged in multiple family developments.

#### Choice of Use (for example, home based business, live / work)

- 1 Combining work and living space reduces the carbon footprint for residents who maintain separate workplaces.
- 2 The choice of office, retail, commercial or residential use is allowed on the ground floor of multiple family developments from 4-6 storeys.
- 3 A minimum 3.6 metre ground level floor to floor height is recommended where choice of use is permitted.
- 4 The choice of use for each ground oriented unit can vary within a single development.
- 5 Units with residential neighbours on both sides should be restricted to retail or office use with daytime hours of operation with the following guidelines, where possible:
  - Signs and / or identification of the commercial spaces must respect the residential use within the building.
  - Signs should be restricted to the front garden wall or painted signs on entry doors and / or windows, and only low watt lighting is permitted.
  - Business use should not extend into the front setback.
- 6 The nature of the business choice of use must be compatible with the building's residential uses, and therefore businesses whose hours of operation extend into the evening are only allowed on corner units with the following guidelines:
  - Signs and / or identification of the commercial spaces must respect the residential use within the building.
  - Signs are restricted to the front garden wall or painted signs on entry doors and / or windows.
  - Lighting cannot produce glare into neighbouring residential units.



**Private Outdoor Space**

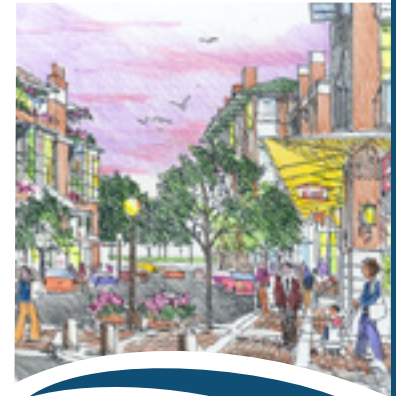
- 7 Access to private patios, decks and gardens is encouraged as an amenity for all ground level units, and to continue the neighbourhood pattern of street oriented private front gardens.

**Entrances**

- 8 Ground oriented street fronting units are highly encouraged to have private front entrances, visible from the street.

**Landscape**

- 9 Garden walls or fences are encouraged along the front of the property, including individual garden gates for each ground oriented unit.
- 10 Landscape materials should facilitate best stormwater management practices and the potential for urban agriculture, where possible.
- 11 Exterior lighting at front gates and entrances should not produce glare to neighbours.



**Form and Character**

**Entrances**

- 12 The design of multiple family entrances should be prominent on the street, welcoming, and provide a sense of identity to the development.
- 13 The incorporation of landscape and seating into the entry design is encouraged.
- 14 Entrances should include waiting areas and a clear view to the street.



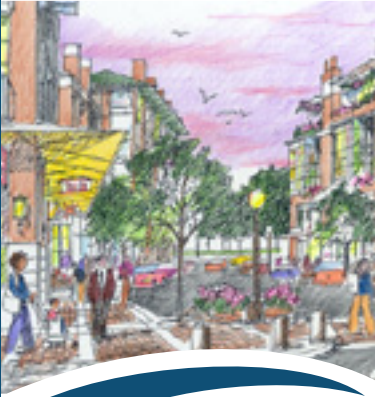
Private front gardens, gates, seating areas and spaces for gardening are valuable amenities in multiple family developments.

**Materials**

- 15 Building materials should be durable, attractive and, where possible, contain recycled material.
- 16 Natural building materials such as wood, stone, brick and metal are encouraged.
- 17 Earth toned colours are recommended for the overall appearance of multiple family developments.
- 18 Bright or highly saturated colors are most appropriate for architectural features or unique elements.

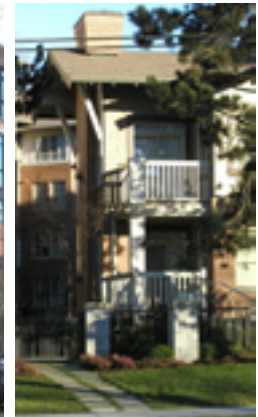
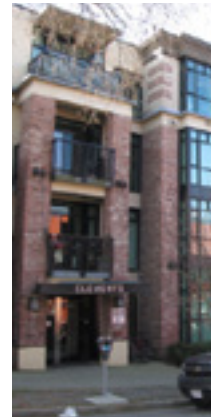


A distinctive entrance can create a residential identity.



**Balconies**

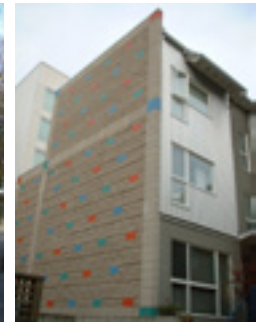
- 19 Balconies are recommended to provide additional private outdoor space.
- 20 The size of balconies should promote usability. A minimum 2.1 metres depth is recommended.
- 21 Balcony locations should respect neighbour’s privacy.
- 22 Access to sun increases the usefulness and enjoyment of balconies.
- 23 Materials and details, including guardrails, should be consistent with the design intent of the townhouse.



Balcony design should enhance and complement the overall building design.

**Side and Rear Elevations**

- 24 Side and rear elevations should be attractive and contribute to the overall design of the development.
- 25 High quality materials used on the front elevation should be extended along side elevations to avoid a ‘wallpaper’ effect, where materials appear pasted onto front elevation.



Rear elevations should be consistent with the overall design.

The design of blank walls should be attractive wherever possible.

**Signs and Addresses**

- 26 The quality of the signage, including the street address, contributes to the identity of the development.
- 27 It is important that design elements visible from the street are complementary, creating a cohesive streetscape.



Materials, details, lighting and signs are integrated into a cohesive design.

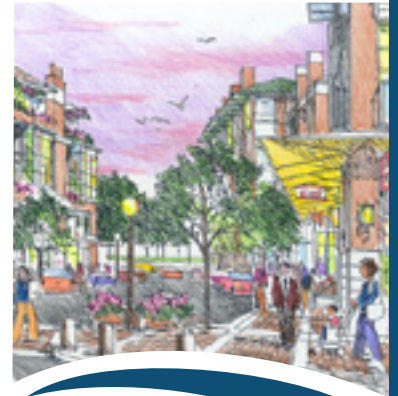
## Landscape

### Garden Walls, Fences and Gates

- 28 Integrating garden walls, fences and gates into higher density developments continues traditional neighbourhood patterns.
- 29 Landscape elements provide a transition from public to private space.
- 30 Edging multiple family developments with garden walls or fences can increase security by clearly identifying the public and private realms.

### Common Outdoor Space

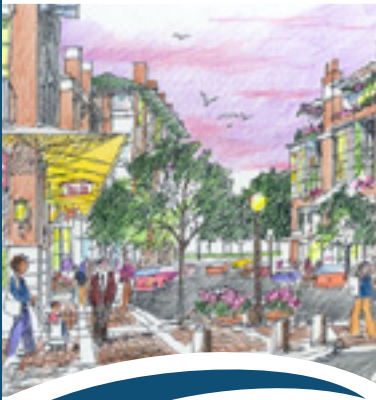
- 31 Common outdoor spaces should have access to daylight from 10 am to 2 pm.
- 32 A balance of hard and soft landscaping is recommended for all weather use.
- 33 Common outdoor space should be visible from dwelling units to ensure surveillance by residents.
- 34 Both noisy and quiet activities should be accommodated within the design.



Well designed common outdoor spaces increase livability in multiple family developments.



Garden walls, fences and gates should contribute to an attractive streetscape.



### Planting

- 35 Native plant material is recommended to minimize irrigation requirements.
- 36 A variety of plant material is recommended to provide colour, summertime shading, texture and variety.
- 37 Layering planting with screens, fences and garden walls is encouraged.



Plant material should contribute colour and variety year round.

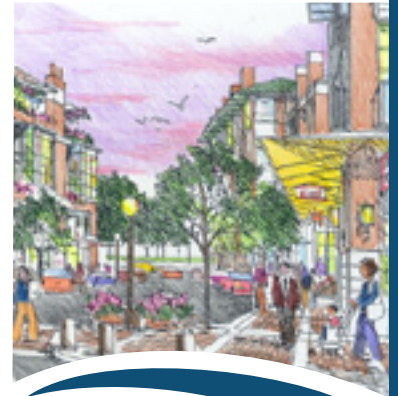
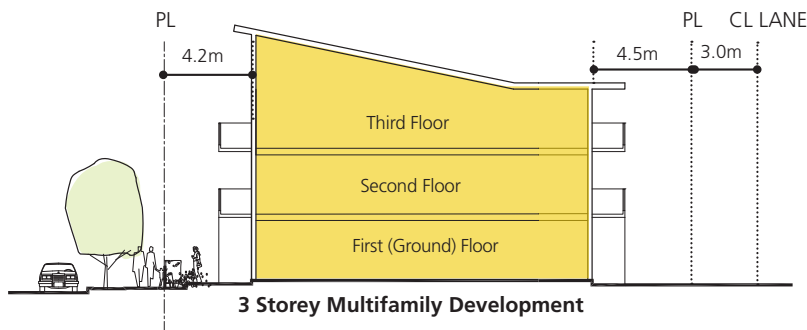
### Exterior Lighting

- 38 Multiple family developments have a range of exterior lighting requirements, including common and private entrances and outdoor spaces, pedestrian pathways, and landscape lighting.
- 39 Lighting design should consider safety and aesthetic requirements.
- 40 Lighting design should be consistent with the overall architecture and landscape design, and incorporate energy saving products where possible.
- 41 Light levels should be appropriate for visibility and safety, and designed to reduce light spillage.
- 42 Bright, glaring lights are to be avoided.

## Massing and Setbacks

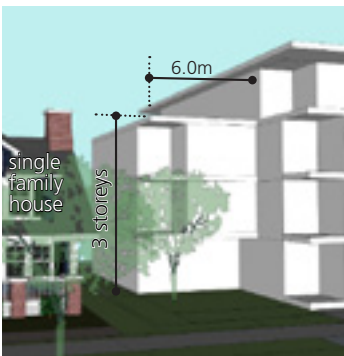
### General

- 43 All residential developments to be set back a minimum 4.2 metres from the front property line. Balconies and canopies are allowed within the setback.
- 44 Rear setbacks of 7.5 metres from the centre line of the lane to allow access to light and privacy. Where there is no lane, a setback of 7.5 metres from the rear property line is recommended.



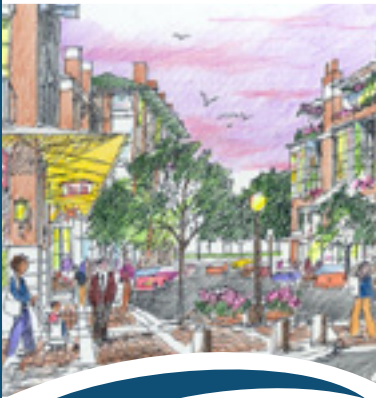
### Transition to Lower Density Neighbours

- 45 Residential developments with four or more storeys should respect the scale and location of adjacent lower density neighbours.
- 46 Increasing the front setback for the portion of the building within 6.0 metres of the interior side yard, adjacent to a lower density development, is recommended.
- 47 Reducing the building height to three storeys adjacent to lower density developments within the 6.0 metre portion of the building is recommended.



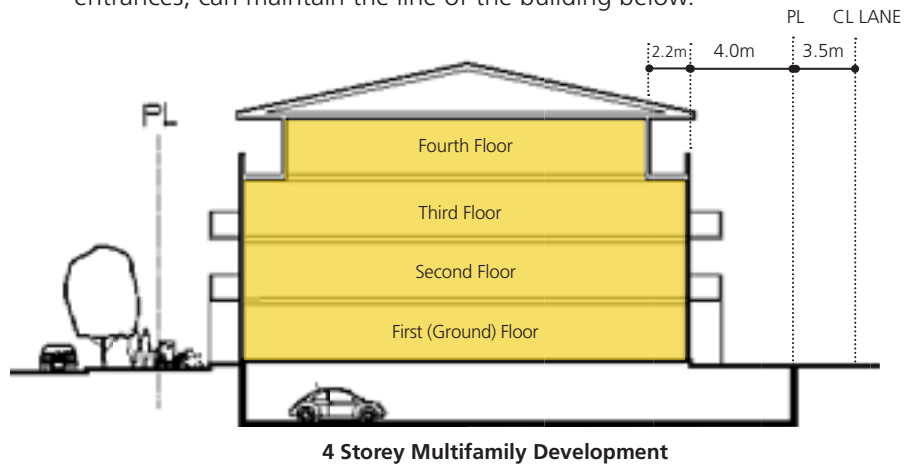
Higher density developments should respond to the heights and setbacks of lower density neighbours.

Three storey heights adjacent to walkways is recommended. Portions of the buildings with more than three storeys should be stepped back 2.2 metres.



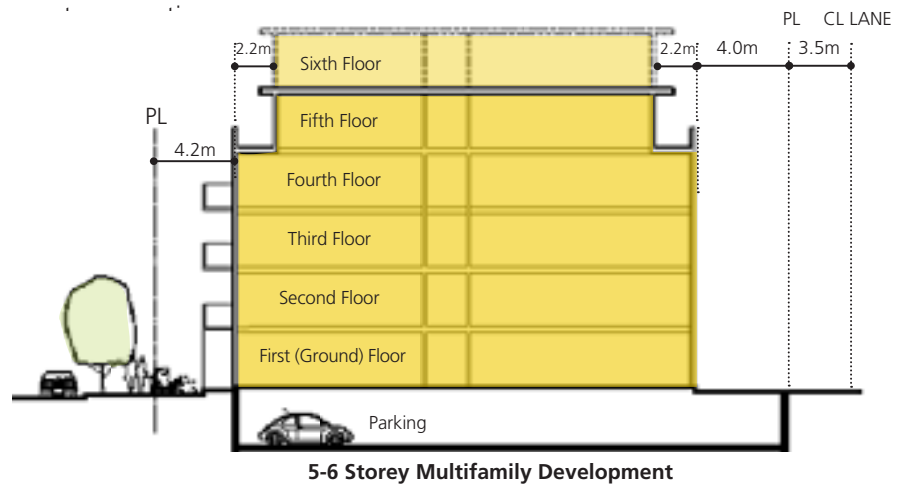
### Development Up To 4 Storeys

- 48 Four storey developments are generally required to set back the fourth storey an additional 2.2 metres.
- 49 Portions of the fourth storey adjacent to neighbouring developments, at street corners, or to emphasize building elements such as stairwells or entrances, can maintain the line of the building below.



### Development Up To 6 Storeys

- 50 Five and six storey developments are generally required to set back the fifth and sixth storey an additional 2.2 metres from the front property line.
- 51 Portions of the fifth and sixth storey adjacent to neighbouring developments, at street corners, or to emphasize building elements such as stairwells or entrances, can maintain the line of the building below.
- 52 Three storey heights are recommended adjacent to pedestrian walkways. Increased building heights can be set back 2.2 metres from the three



note: roof forms, balconies and other architectural elements are for illustrative purposes only



## 7 Building Design Guidelines Neighbourhood Development

### 7.1 Small Scale Residential Infill up to 3 Storeys

Innovative, small scale ground oriented developments, including single family houses and duplexes, increase neighbourhood housing choice.

#### Building Massing

- 1 Massing, footprints and heights should complement existing neighbourhood forms including residential setbacks.
- 2 Where there is a uniform front yard setback, infill buildings should respect this setback and fit into the streetscape.
- 3 Create a transition in building heights if the new development is higher than the neighbours.
- 4 Create a transition in building widths if the new development is significantly larger than adjacent buildings by visually dividing the building width into smaller vertical elements that approximate the width of the neighbours.
- 5 Building forms should articulate topography by sloping or stepping down to provide access to light and privacy onto side yards.



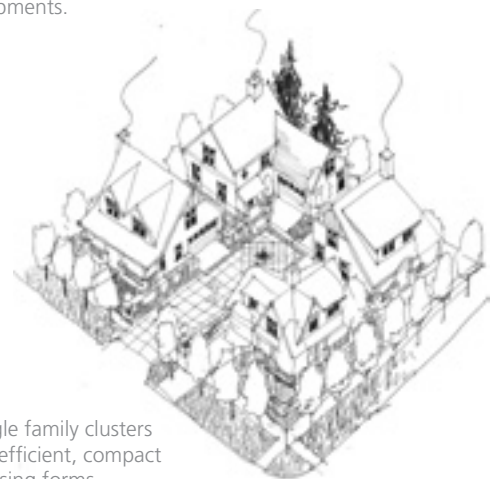
Small lots are opportunities for design innovation.



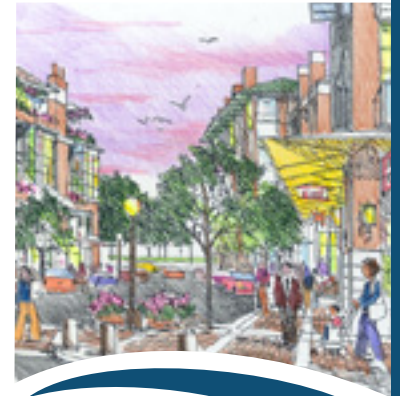
Shared driveways and walkways provide connections within higher density developments.

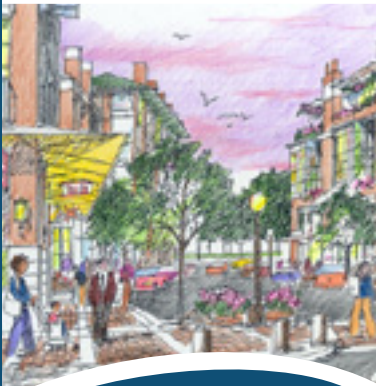
#### Form and Character

- 6 New developments can introduce new architectural styles to a street while respecting the scale, mass and pattern of the street.
- 7 Where the architectural style and building form is new to the street, good architectural design including high quality materials and detailing increase neighbourhood acceptance.
- 8 Off street parking accessed from the lane is recommended.
- 9 Landscape design and materials should complement the overall building design, contribute to the public realm where they meet the street, and complement the neighbourhood.
- 10 Strategies to maximize rainwater retention, including the use of permeable surfaces and minimum building footprints are encouraged.
- 11 Discourage solar shading of neighbouring buildings with infill development, and encourage light penetration into units.



Single family clusters are efficient, compact housing forms.





## 7.2 Coach Houses

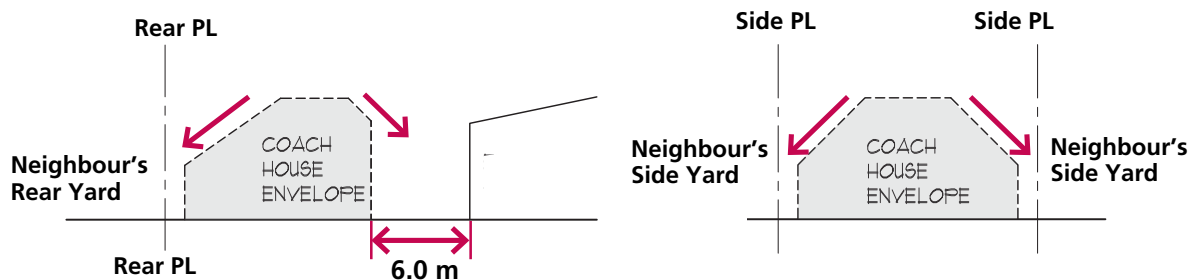
Coach Houses are appropriate infill prototypes on lots with existing single family houses. They can provide additional space for families, extended families, home office use or provide rental income. Respecting adjacent residents' access to light and privacy determines their success within the neighbourhood.

### Building Massing

- 1 Massing, footprints and heights should complement existing neighbourhood patterns and forms.
- 2 Coach House height should not exceed two storeys.
- 3 The principal house should generally be higher than the coach house.
- 4 Coach house massing should slope down to preserve access to light and privacy for neighbours and the principal dwelling.
- 5 6:12 is typically recommended for sloped portions of the building mass.
- 6 8:12 degrees is recommended for portions of the coach house adjacent to the rear property line with no lane.
- 7 6.0 metres is the recommended minimum separation between the principal house and the Coach House.

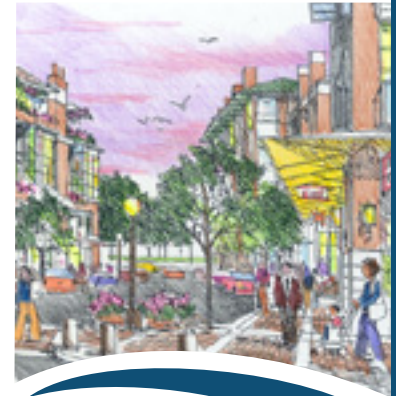


Coach houses can provide valuable living space with minimum impact on neighbour's privacy and private outdoor space.



## Form and Character

- 8 Infill housing prototypes should increase housing choice while preserving neighbourhood patterns.
- 9 Coach Houses should enhance the character of the lot through high quality design, materials and detailing.
- 10 Design innovation is encouraged. The architectural style of the Coach House is not limited to the style of the principal dwelling but should respect neighbourhood context.
- 11 Well proportioned facades, openings and architectural elements that are appropriately scaled to the overall building form are encouraged.
- 12 Driveways should be shared wherever possible to maximize on street parking opportunities and sidewalk continuity, and to minimize the amount of paved surfaces.
- 13 Access and entrances to Coach Houses should be clearly defined and may include front gates and illuminated house address.
- 14 Strategies to maximize rainwater infiltration, including the use of permeable surfaces and minimum building footprints are encouraged.
- 15 The design of exterior lighting should carefully consider privacy between the Coach House and principal house. Pathways and driveways should be illuminated at night without light spilling into adjacent properties.
- 16 A minimum private outdoor space of 15 m<sup>2</sup>, directly accessible from private indoor space is recommended for Coach Houses.
- 17 Ground level private outdoor spaces are preferred to balconies and roof top decks to maximize access to privacy and light for adjacent properties.



Selected heritage elements including bay windows, simple, steeply pitched roof forms and painted wood cladding are appropriate for heritage style Coach House designs.



Well designed, contemporary forms including strong, simple roof shapes, large areas of glazing, deck and patio spaces that extend interiors, and simple building forms can be appropriate options for Coach Houses.

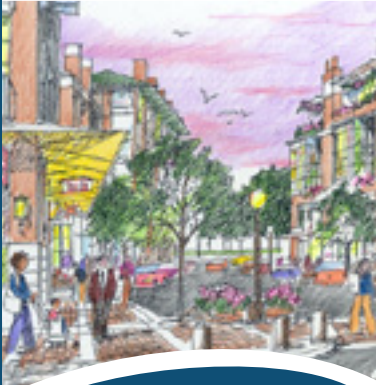


Gabled roofs, wood cladding, generous trim proportions, simple building forms and careful landscaping allow coach houses to fit into existing neighbourhoods and complement traditional and heritage style properties.



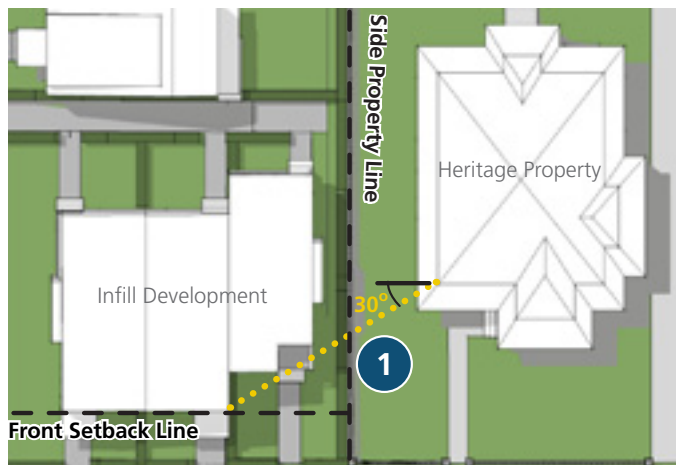
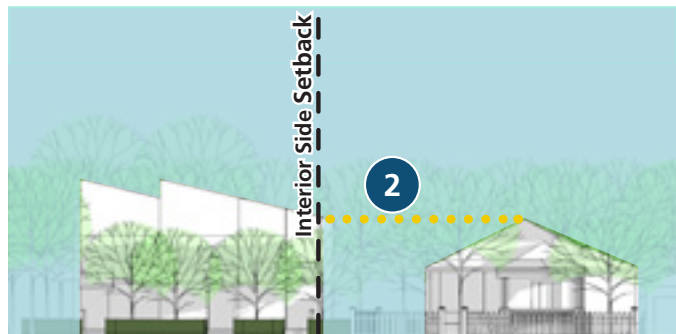
Provide the Coach House with a presence on the front sidewalk to include it in the neighbourhood and allow clear identification.

### 7.3 Heritage Areas Developments Adjacent to Residential Heritage Properties



Registered heritage properties are important determinants of neighbourhood character and value, to be protected and maintained as the Harewood area develops. These guidelines are intended to minimize the impact of adjacent developments on these landmark properties. New developments are not expected to reflect or complement the architectural style of the heritage developments. They are, however, encouraged to consider the scale and form of their heritage neighbour, providing some ‘breathing space’ for these local assets and continuity along the streetscape.

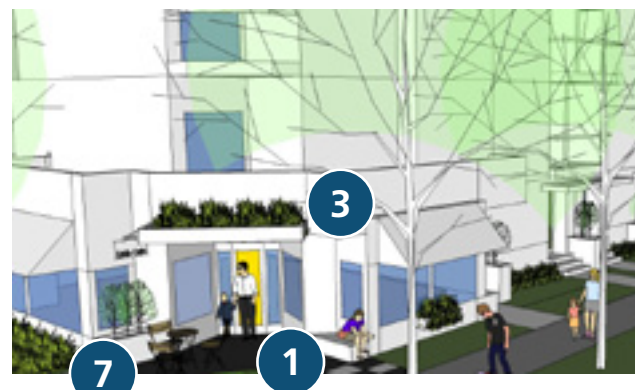
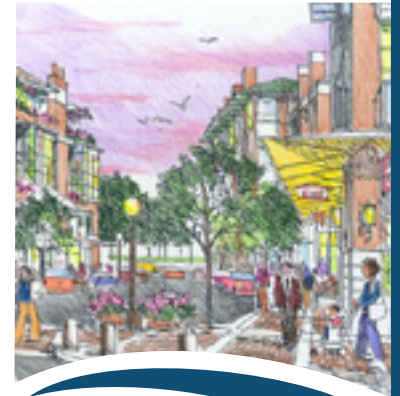
- 1 Increase the front setback of the portion of the building, determined by a line extending from the nearest corner of the heritage building at 30° to the front elevation towards the new development. Porches, steps and overhangs are allowed to project into this setback.
- 2 The building height at the interior side setback line should not exceed the highest point of the heritage building’s main roof form.
- 3 Building facades, especially elevations facing heritage properties, are encouraged to be layered and articulated. Blank or flat facades that are in direct contrast to detailed heritage architecture are discouraged.



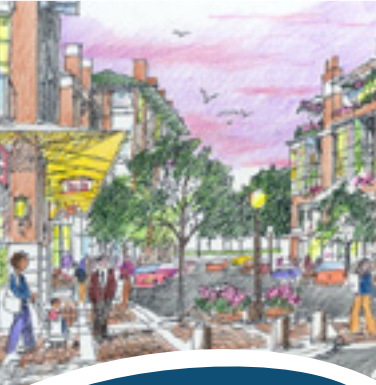
## 7.4 Local Service Centre

Introducing small scale commercial developments into residential neighbourhoods supports walkability and social connections. Corner stores and coffee shops, for example, can reinforce a neighbourhood identity and provide a convenient place to shop and gather.

- 1 Incorporate a recommended minimum 6.0 m<sup>2</sup> public space at the commercial entrance to allow space for seating, planters and gathering.
- 2 Consider providing a distinct paving material at the entrance to contrast with the adjacent sidewalk.
- 3 Incorporate hard and soft landscape into the commercial facade and entrance to blend this commercial use into the neighbourhood context and allow it to be a good neighbour.
- 4 Set back the commercial facade 1.0 metre from the front property line to clearly define the corner location and differentiate commercial and residential use on the streetscape.
- 5 Orient shopfront windows away from adjacent residential use to maintain privacy.
- 6 Incorporate the street name or other neighbourhood identity into signage and / or hard landscape.
- 7 Integrate outdoor furniture including recycling, garbage, seating and planters into the commercial shopfront design.



## 8 Street Design Guidelines



The intent of the street design guidelines is to improve the streetscape, enhance the public realm, develop community links, and create a stronger sense of place by balancing safe and efficient bicycle and pedestrian connections with safe and efficient vehicle use. A successful pedestrian network connects public open spaces, parks, neighbourhood amenities and shopping districts, which reduces dependence on the car. Specific recommendations for selected neighbourhood streets follow the general guidelines and have been designed according to the principles of 'Complete Streets', addressing all forms of transportation while being sensitive to the context of adjacent development form, land uses and the environment.

### 8.1 General Street Design

#### Road Classifications

The road network plan for Harewood is based on a road hierarchy system that reflects the service function of each roadway, in accordance with the City of Nanaimo's roadway classifications. This hierarchy includes Major Roads (Arterial and Major Collector Roads) and Minor Roads (Neighbourhood Collector, Local Roads and Lanes).

#### Guidelines

- 1 Road classification guidelines are as laid out in the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.
- 2 Proposed roads to be laid out as per Map 2 - Pedestrian and Road Network of the Harewood Neighbourhood Plan.
- 3 On street parking to be permitted on both sides of Collector Roads, to support the increased parking demand associated with higher density residential uses and ground level commercial planned on these streets.
- 4 Proposed rights of way for Harewood streets are as laid out in Table 2.1 of the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.

## Water Management and Permeable Surfaces

Rainwater management strategies that increase water infiltration, minimize direct runoff into city stormwater systems and adjacent waterways are expected, and may include rain gardens, bioswales and rainwater collection systems integrated into street and landscape designs.

Permeable paving allows rainwater to penetrate the soil, minimizing runoff and diverting pollutants from the City sewer system. It can eliminate the need for plant irrigation.

### Guidelines

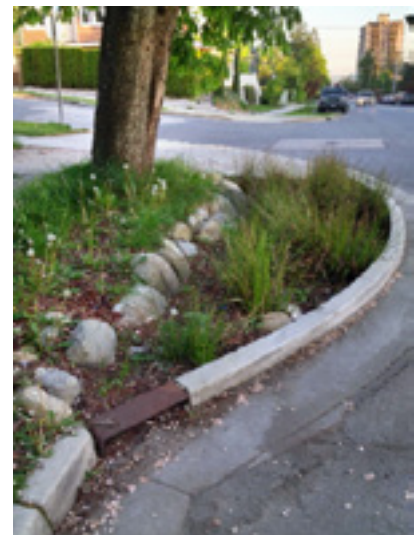
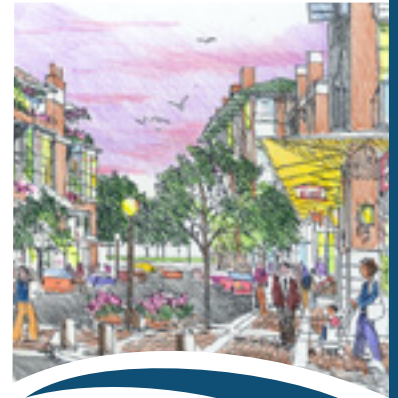
- 5 Consider permeable surfaces on select locations such as off street parking, cafe or restaurant patios, driveways, plazas for special events areas, or specially designated street areas.
- 6 Consider using a 'signature' permeable material throughout the Neighbourhood Plan area to support a neighbourhood identity, to allow for bulk orders, stockpiling of extra material, and ease of maintenance.
- 7 Rainwater and stormwater should be harvested for irrigation and other on site uses.
- 8 Use drought tolerant plants for landscaping.
- 9 Include water management strategies and water retention facilities around community garden sites.

## Sidewalks

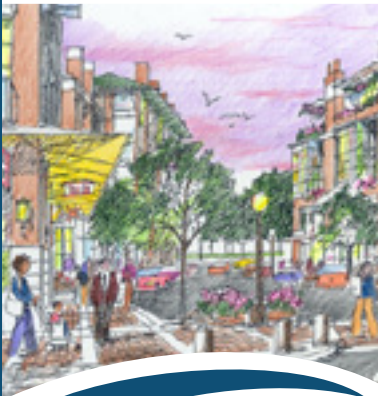
Well designed and generous sidewalks enrich the quality of the public realm. These guidelines support upgrading existing discontinuous sidewalks with a well connected, fine grained, pedestrian network.

### Guidelines

- 10 Ensure continuous sidewalks throughout the neighbourhood.
- 11 Sidewalks should be paved (for example, concrete, brick) and sloped to drain toward a continuous curb and gutter. Sidewalks should be durable, even surfaced and readily maintained or replaced. All surfaces should be smooth and accessible for a wide range of users.
- 12 Consider texture on concrete sidewalks by scoring or sawcutting, or introducing stamped decoration such as neighbourhood logos and motifs into the concrete.
- 13 All sidewalks should be a minimum 1.8 metres wide on Major Roads, 1.5 metres on Local Roads, and should be clear of all obstructions. In blocks with commercial ground floor street frontage, such as Wakesiah



Example of drainage feature in curb extension. Photo credit: Bunt & Associates Engineering



Avenue between Third Street and Fifth Street, and in *Harewood Village*, increase sidewalk width to a minimum 2.8 metres within the public right of way; and with an additional 1.5 metre building setback from the property line, totalling 4.3 metres. Provide special pavers and other features to support development of a high quality pedestrian environment.

- 14 Provide permeable surfaces wherever possible.
- 15 Sidewalks should have curb drops at all intersections, crosswalks and driveways.
- 16 Street trees, street furniture, poles and signage should be accommodated in additional sidewalk width beyond the 1.8 metres.
- 17 Offset sidewalks from the travelway / parking lane to provide a buffer from moving traffic. This boulevard space should be sufficiently wide, 2.5 metres including curbs, to allow for the installation of street trees, landscaping; and street furniture such as benches, bus shelters (with wheelchair pods), potential drainage features and public art.

### Bicycle Paths

Designated bicycle routes ensure safety and allow for smooth flow of bicycle traffic.

### Guidelines

- 18 Designated bicycle lanes to be minimum 1.8 metres wide.
- 19 Clearly define bicycle lanes using painted lines and markings, or through the use of contrasting paving material, particularly at intersections.
- 20 Bicycle routes to be identified with signage and street painting. Major intersections to include pedestrian and cyclist activated crossing lights.
- 21 Complete and connect the on street bike lane and bike way networks with appropriate 'bicycle friendly streets' signage.
- 22 Where appropriate, provide wide 4.3 metre shared curb lanes with 'sharrow' template markings, and / or restricted parking to improve the cycling environment. Sharrows are bike stencils on the pavement and are increasingly being applied where lane widths are not considered adequate or necessary for marked / striped bike lanes.
- 23 Develop 4.0 metre wide asphalt pathway connections to the Nanaimo Parkway at Park Avenue, Bruce Avenue, Ninth Street and Howard Avenue.
- 24 Work with the Island Corridor Foundation to continue the E & N Railway Trail and construct neighbourhood connectors. At the eastern edges of the neighbourhood, connect the future E & N Railway Trail to Seventh Street, Fifth Street, and Fourth Street.



Example of sharrow stencil.



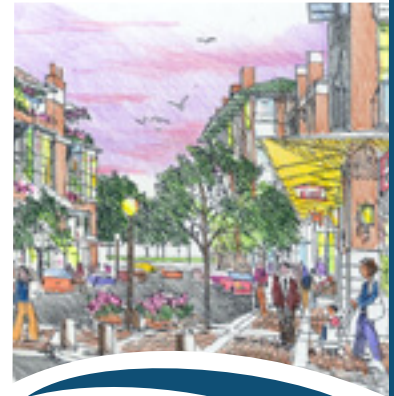
Example of major road bike lanes.



Example of two stage bicycle median crossing.



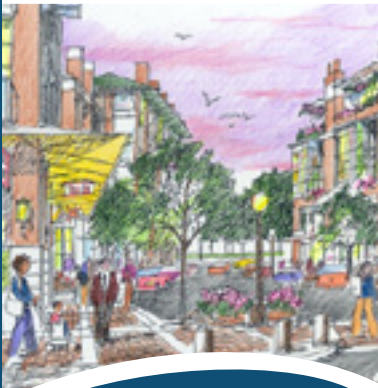
- 25 Develop an off street multi use pathway between Bruce Avenue and Tenth Street, following the creek corridor running northwest to southeast between Bruce Avenue at Ninth Street and Tenth Street east of Park Avenue.
- 26 As part of the Georgia Avenue bikeway, create a new 4.0 metre wide multi use pathway across Harewood Centennial Park, with a new multi modal bridge over the Chase River.
- 27 At all existing traffic signals on the Bicycle Network, retrofit signal hardware to include bicycle push button activation.
- 28 At all new traffic signals installed on the Bicycle Network, install bicycle push buttons as a standard installation.
- 29 Provide two stage unsignalized crossings, with breaks in medians, on Arterial and Major Collector roadways, where feasible.
- 30 Consider installation of coloured bike lane treatments to emphasize vehicle / cyclist conflict zones at intersections.
- 31 Develop a city wide standardized wayfinding signage system and pavement marking standards on all bicycle routes, in keeping with industry standards.
- 32 Develop requirements for bicycle parking and bicycle end of trip facilities for all newly constructed buildings and rezoning sites, and update the City's Zoning Bylaw. Consider adopting requirements for short term (Class I) and long term (Class II) parking.
- 33 Provide short term bicycle parking in the form of bicycle racks at entries to multiple family buildings, in local shopping areas, and at recreation destinations.
- 34 Work with the University Village Shopping Centre and other private property owners in identifying easy solutions to providing more, or better, bicycle parking.
- 35 Provide long term bicycle parking in the form of bicycle storage rooms or cages within multiple family buildings, schools, offices and other places of employment, which require higher levels of security and weather protection.
- 36 Encourage installation of end of trip facilities such as lockers, change rooms and showers at schools, offices and places of employment.
- 37 Improve weather protection for bicycle parking at schools, to be sited near building entries, providing enhanced security through informal surveillance from offices or classrooms.
- 38 Provide secure, weather protected bicycle parking at the VIU Exchange, at the Bruce Avenue and Fifth Street Exchange, and at the Tillicum Lelum timing point on Tenth Street, to support trip chaining.



Example of pedestrian push button installations.



Example of unique and functional short term bicycle parking rack.



- 39 Install free standing 'cluster' bike racks at parks, such as Harewood Centennial Park, where cyclists may travel for outings and will need parking within site of other park users.
- 40 Treat bicycle racks as public art opportunities while maintaining lock up functionality.

### On Street Parking

On street parking is incorporated into residential and commercial streets for convenience and to facilitate traffic calming. Parked cars provide a buffer between moving traffic and pedestrians.

#### Guidelines

- 41 On street parallel parking is proposed on both sides of all streets, except where grading or other site specific variations restrict available road width.
- 42 Design and reduce parallel parking lanes to a minimum of 2.3 metres to 2.35 metres to maximize sidewalk width and boulevard / landscaping opportunities; on Local Roads, consider 2.2 metres.
- 43 Provide corner bulges to minimize crossing distances and to facilitate maximum visibility of pedestrians.

### Off Street Parking and Driveways

Off street parking and driveways should contribute to an attractive and functional streetscape.

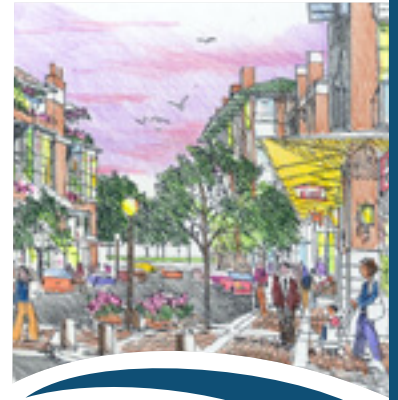
#### Guidelines

- 44 See Off Street Parking Guidelines section for detailed off street parking guidelines.
- 45 Limit the width of each driveway to 3.0 metres for single family homes, 6.0 metres for multiple family developments, and 7.0 metres for commercial accesses. Pave all driveways in a consistent manner along entire length of each street.
- 46 Upgrade substandard driveways to meet guideline standards.
- 47 Access to off street parking areas from the rear lane is recommended in all future developments. Where lane access is not available, off street parking may be accessed from the street, provided crossing widths and the impact on pedestrian and cycle routes are minimized.

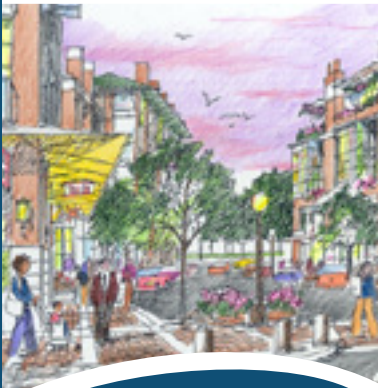
## On Street Pedestrian Routes

On street pedestrian routes provide an important network of connectivity throughout the neighbourhood.

- 48 Provide on street pedestrian facilities with sufficient width for all types of activities including walking, standing and social interaction. These areas should be designed with an emphasis on user comfort, convenience and safety.
- 49 Separate / buffer pedestrian facilities from vehicular traffic on higher volume streets with boulevards and / or on street parking.
- 50 Incorporate design features that accommodate wheelchairs, scooters, as well as hearing and vision impaired users.
- 51 Develop all Arterial and Collector Roads within the Harewood neighbourhood with two sidewalks, at least 18 metres wide.
- 52 Develop all Local Roads with two sidewalks, at least 1.5 metres wide.
- 53 Create *Pedestrian Priority Zones* along the ground floor commercial frontage planned on Wakesiah Avenue, Bruce Avenue and Eighth Street, by creating additional sidewalk width to support high pedestrian demand, but also to incorporate street design features such as benches, public art, etc.
- 54 Provide supporting street furniture that supports walking, such as benches, drinking fountains, and way finding signage, particularly within the *Pedestrian Priority Zone* and around commercial nodes and transit exchanges / stops.
- 55 Install street trees in the boulevard to provide shade and a pleasant environment for walking.
- 56 Encourage on street parking to provide additional buffer between pedestrians and vehicle traffic, and in particular within the *Pedestrian Priority Zone* to support the ground floor commercial businesses.
- 57 Incorporate traffic calming features on proposed neighbourhood bikeways of Harewood Road, Georgia Avenue and Hillcrest Avenue, to reduce vehicle traffic volumes and speed and improve pedestrian safety on these routes. Focus specialized pedestrian / cyclist crossing treatments on these bikeways.



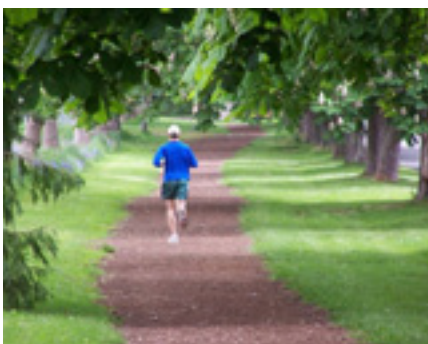
Example of neighbourhood bikeway route with traffic circle to calm vehicle traffic.



### Off Street Pedestrian Routes

A network of off street pathways can reduce walking distances to key destinations within the neighbourhood, and create a pleasant recreational walking network.

- 58 Continue to support the Island Corridor Foundation to develop the E & N Rail Trail with connections to adjacent pedestrian linkages consistent with the Pedestrian Network Plan.
- 59 Provide new or improved pedestrian connections to the Nanaimo Parkway Trail in South Harewood.
- 60 Develop a common way finding signage system. Pedestrian way finding signs help users identify the best pedestrian routes to key destinations.
- 61 Promote responsible walking and cycling behaviour on shared multi use pathways.
- 62 Plan for new pedestrian connections through future subdivisions by requiring developers to dedicate public pathway rights of way, to connect to adjacent neighbourhood streets and transit stops.
- 63 Develop and maintain off street pedestrian trails / pathways with a minimum width of 2.0 metres when not shared with cyclists, and a new minimum paved width of 4.0 metres when shared with cyclists.
- 64 Install a new multi use pathway bridge across the Chase River through Centennial Park; consider the feasibility, environmental impact and cost of new pedestrian bridges across the Chase River at Eighth Street and Douglas Avenue.



Examples of paved and unpaved pedestrian pathways / trails.

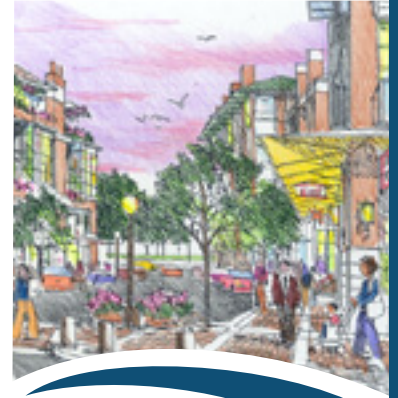
### Pedestrian Street Crossings

Clearly identified pedestrian crossings, including pedestrian activated flashing crosswalks (in pavement, overhead, or sidemounted) are recommended at all major streets and intersections, where higher pedestrian volumes exist.

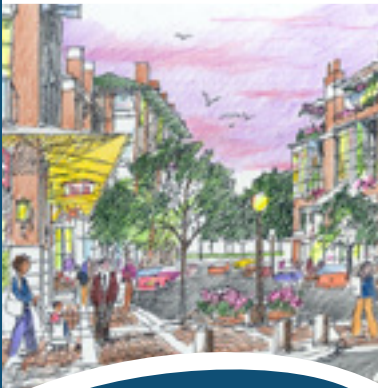
### Guidelines

- 65 Incorporate white markings, reflective paint, contrasting paving, lighting and signage to increase pedestrian visibility during night and day.
- 66 Provide curb extensions at all intersections of Major Roads with other Major Roads or Local Roads, to reduce pedestrian cross distances, improve visibility of pedestrians to drivers, and provide an opportunity for landscaping, drainage features, street furniture or public art.

- 67 Introduce landscape into curb bulges to improve the overall appearance of the street and to provide a buffer between pedestrians and traffic.
- 68 Provide raised medians on all major roads, between intersections, to manage access and allow safer two stage pedestrian and bicycle crossings. Medians should be 3.7 metres to 3.8 metres wide, including curbs, to permit street trees, landscape, or potential drainage features, where appropriate.
- 69 Provide wheelchair letdowns on all four quadrants of all intersections, regardless of whether they are signalized or not.
- 70 Consider installation of tactile concrete treatments at wheelchair letdowns for the visually impaired, particularly in the *Pedestrian Priority Zone*, parks and commercial zones where higher numbers of these types of pedestrians are expected to be present.
- 71 Ensure all intersections and mid block crossings are well lit.
- 72 Retrofit existing traffic signals and plan new traffic signals to include pedestrian audible countdown timers at signalized intersections, which have been proven to increase pedestrian safety.
- 73 Provide marked crosswalks at all signalized intersections.
- 74 Implement marked crosswalks at unsignalized intersections or at mid block locations as required according to site specific operations and safety conditions, but consider more frequent marked crossings within the *Pedestrian Priority Zone*.
- 75 Provide 'Special Crosswalk' treatments (flashing amber side mounted poles) or full Pedestrian Signals at high demand pedestrian crossing locations, based on meeting required installation warrants.
- 76 Provide standard industry signage and pavement markings for all pedestrian crossing, except in the *Pedestrian Priority Zone* and on neighbourhood bikeways, where unique pavement treatments should be considered and signage should not be necessary given slower traffic speeds.
- 77 Install curb extensions at all Arterial and Collector intersections to reduce pedestrian crossing distances and increase visibility for drivers. Develop curb extension designs with minimum curb radii to accommodate design vehicles, taking into account their relative frequency and ability to sweep into concurrent and / or opposing lanes of traffic. This will increase safety for pedestrians by reducing crossing distances and increase space for landscaping and drainage features within curb extensions.
- 78 Provide mid block two stage median crossings with curb extensions at major pedestrian route crossings where feasible and warranted.



Example of a curb extension at an unsignalized intersection with signage and pavement markings.



### Street Trees and Landscape

Enhanced landscapes within the public realm add beauty to the community, increase pedestrian comfort, add shade and visual relief to paved streets, and support the creation of habitat corridors through the neighbourhood.

#### Guidelines

- 79 Street trees provide screening, pedestrian scale and colour to neighbourhoods. A single row of trees is recommended on both sides of all streets. Trees should be regularly spaced (6.0 metre - 8.0 metre spacing is optimum), and should be planted in appropriate growing medium with sufficient space for roots, using root barriers or other technology to prevent root damage to sidewalk paving and tree grates.
- 80 Trees to be set within tree grates on commercial streets. Consider a 'signature' Harewood design for tree grates.
- 81 Special varieties ('signature') of tree species are recommended as gateway elements or to give local streets their own identity.
- 82 In areas where ocean or mountain views are to be maintained, use street trees with a compatible compact form.
- 83 Landscapes should be generous and expansive in scale and form, and should focus on native plants and drought resistant species. Reliance on species that require frequent irrigation and maintenance should be avoided. If irrigation is necessary, drip irrigation and other water efficient irrigation systems should be used. Plant diversity, plants that are native to the region and microclimate, and those which naturally grow together and are self sustaining (for example, reseed and spread without much maintenance) are preferred.
- 84 Provide planting that complements the character of the built environment. Use flowering shrubs and trees, including edible fruit and nut trees, where they can be most appreciated, such as adjacent to walks, recreational areas or as framing devices for building entries, stairs and walks. Provide planting designs that work with pedestrian movement or views.
- 85 Community garden spaces should be incorporated into the public realm wherever possible, including curb bulges and boulevard areas. Areas designated as community gardens should be organized and designed in such a way that they remain aesthetically pleasing during non-growing seasons. Organized community groups could be given responsibility for assigned garden plots and food collection.



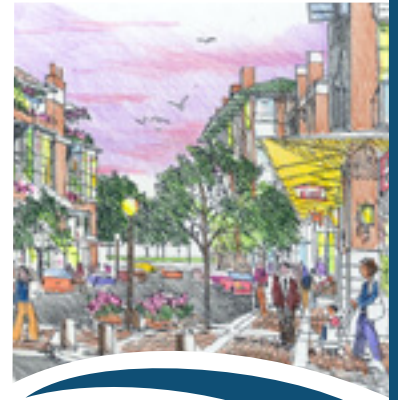
- 86 Rainwater retention areas and rainwater collection cisterns are recommended.
- 87 Consider planted traffic circles within Neighbourhood Collector and Local Road intersections to provide vistas of lush landscaping, and for traffic calming.

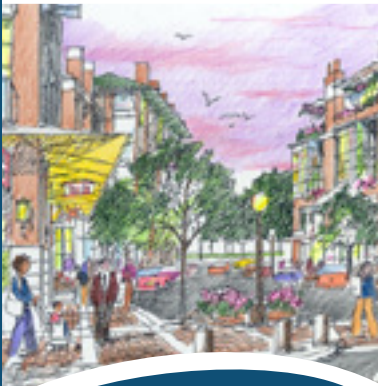
### Street Furnishings

Street furnishings contribute to street character, encouraging neighbourhood street life, and can be used to enhance / develop a neighbourhood theme.

### Guidelines

- 88 Street furnishings include benches, bicycle racks, garbage and recycling receptacles, information signs and / or kiosks, banners and lighting fixtures.
- 89 Avoid lighting options that contribute to light pollution. Consider enhancing street character within 'Pedestrian Priority Zones', as identified in Figure 8, through the integration of decorative and pedestrian scale street lighting. Pedestrian level lighting could be incorporated into street furniture, including benches, bus shelters and planters, at a recommended height of 0.75 metres.
- 90 Community gardens should include benches, arbours, decorative fences and garden storage sheds, garbage, recycling and compost receptacles.
- 91 Locate benches to avoid conflict with planted material, pedestrian circulation and adjacent residences.
- 92 The design of street furniture could be developed as a local street art program.
- 93 Bike racks, preferably weather protected, to be located wherever possible throughout the public realm.



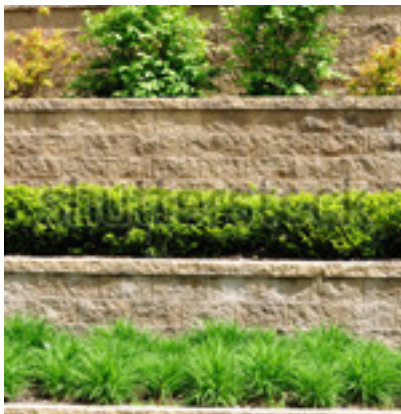


### Public Art

Public art provided by local artists, or neighbourhood inspired art, creates local identity. Public art can include large and impressive gateway elements, or smaller scale pieces that enrich and add interest to the public realm.

### Guidelines

- 94 Gateway elements are potentially free standing landmark artworks, located on public plazas, at entrances to public parks and open spaces, or marking the intersections of important streets.
- 95 Promote neighbourhood art installations under the Volunteer in Parks (VIP) program. Public art could be repeated through the neighbourhood to provide a sense of place, including street furnishings, banners and signage, pavement patterns and mosaics, garbage receptacle design, fountains, rainwater storage devices, manhole covers, street signs, benches, retaining walls, surface drainage channels, light standards and bike racks.
- 96 Durable materials are highly encouraged and help to ensure a long life. Moving parts should be located well above reach for public safety and resistance to damage.



### Fences and Walls

Fences and walls that are part of the urban realm should be carefully considered.

### Guidelines

- 97 Incorporate planting into retaining walls adjacent to sidewalks.
- 98 Fences should incorporate materials and colours that contribute to the public realm.



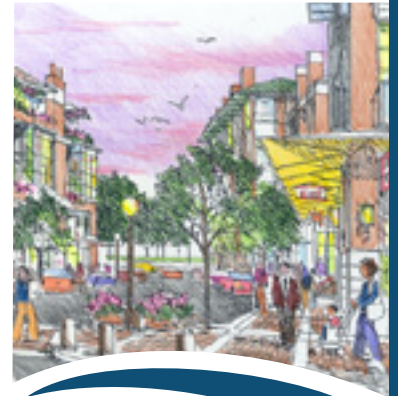


## Bus Stops / Transit Shelters

Good bus stop location and design can significantly enhance the transit experience, and provide an opportunity for signage and art.

### Guidelines

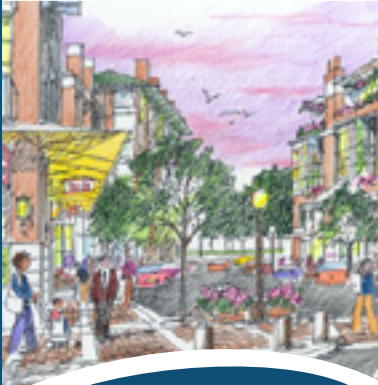
- 99 Bus stops along roadways with transit service should be provided at 150 metre minimum, and 400 metre maximum spacing on higher density Corridors, and at 400 metres maximum in lower density parts of the neighbourhood. Too many stops on a route will slow down bus service, reduce efficiency and impact passenger comfort and should be avoided; however, more frequent stops should be provided on the higher density mixed use Corridors in Harewood where passenger demand will be concentrated.
- 100 Bus stops are generally best located at the 'far side' of intersections rather than 'near side' or mid block, for operations and safety reasons.
- 101 At locations where on street transfers are anticipated (for example, Bruce Avenue and Fifth Street), special consideration should be given to coordinating bus stop placement with passenger transfer movements, so that pedestrian crossing requirements are minimized.
- 102 Bus stop curb extensions or 'bus bulges' should be considered in *Pedestrian Priority Zones* at busier stops to avoid buses having to merge back into traffic and to provide more pedestrian queuing space, even if this creates delays for regular vehicles.
- 103 All bus stops should be designed to be accessible for people with disabilities, with wheelchair pads.
- 104 The first and last part of every transit trip is a walking trip. Pedestrians should have safe and convenient access to transit stops, supported by crosswalks and wheelchair let downs on adjacent streets.
- 105 Busier bus stops should be provided with shelters, benches and schedule information, particularly those located along the Corridors where higher densities are planned. All transit stops should be designed with adequate space for a pedestrian waiting area and accessible to pedestrians with disabilities.
- 106 Bus stops should be adequately lit for passenger safety and security.
- 107 Pedestrian routes to bus stops should be provided including marked crosswalks.
- 108 Consideration should be given to creating a custom design for bus stop furniture (shelters, garbage receptacles, info kiosks) particularly in the *Harewood Village* area, to create unique neighbourhood character. Transit shelter design should be consistent or compatible with other street furnishings.



Example of bus stop curb extension or 'bus bulge' which results in more space for landscape, benches and shelters.



Examples of custom information kiosks and bus shelter designs.



### Weather Protection

Pedestrians need protection from our rainy west coast climate.

#### Guidelines

- 109 Require continuous weather protection such as canopies, awnings, and overhangs for new development with commercial uses at grade.
- 110 Appropriate forms of weather protection include fabric awnings, glass canopies, and solid overhangs. Arcades are discouraged.
- 111 Awnings, overhangs and canopies along commercial streets improve the public realm by providing human scale, street character and identity to individual buildings and weather protection for pedestrians.



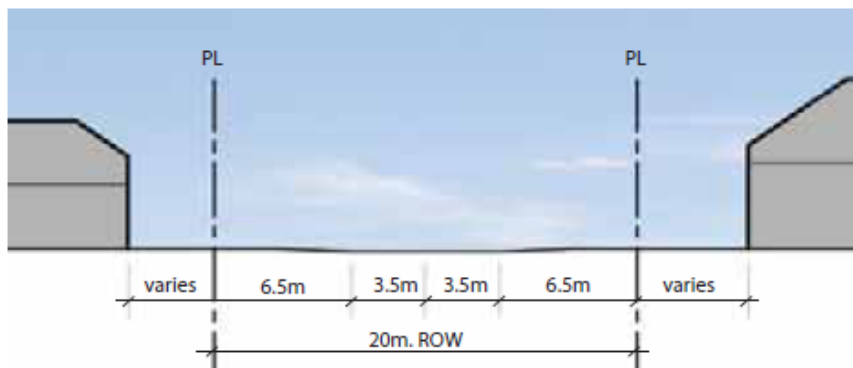
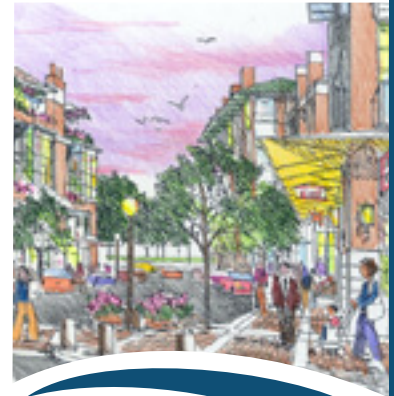
## 8.2 Enhanced Local Roads

### Existing Conditions

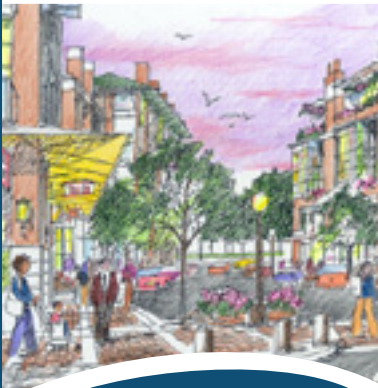
The pedestrian realm of the Harewood Neighbourhood Plan area is under developed and needs connection. Most neighbourhood streets lack conventional pedestrian features such as sidewalks, street trees, curbs and crosswalks; and a large number of residential driveways exit directly onto the street. Residential parking extends from the road onto adjacent properties. In many cases, streets become walkways.

### Guidelines

The intent of these design guidelines is to create a network of Enhanced Local Streets to provide pedestrian friendly connections throughout the neighbourhood as development and redevelopment occurs, or through local improvement initiatives. The incremental addition of street trees, continuous sidewalks, crosswalks with curb drops, and planted boulevards and rainwater treatments will create a lush, green, pleasant and walkable environment for residents and visitors.



Section of typical local street (dimensions are approximate and may vary)



In addition to the recommendations set out in the General Street Design Guidelines, the guidelines for Enhanced Local Streets with street rights of way of 20 metres are as follows:

- 1 Improve safety for both pedestrians and cyclists.
- 2 Increase urban green space and improve air quality through the addition of street trees, landscape and boulevards on all streets.
- 3 Provide opportunities for community landscapes on boulevards, curb bulges, traffic buttons and within extended rights of way. This may be provided through new developments, community participation, or City initiatives.
- 4 Regulate driveways that connect to private properties by implementing requirements for driveway width and materials.
- 5 Queuing streets are recommended for residential streets with lower traffic volumes.
- 6 Where drainage ditches are present, upgrade and enhance the drainage areas with efficient, natural rainwater swales.

A **queuing street** reduces the pavement width and travel speeds by providing one travel lane for traffic flowing in both directions. One of two oncoming vehicles must pull into the parking lane to allow the other to pass.

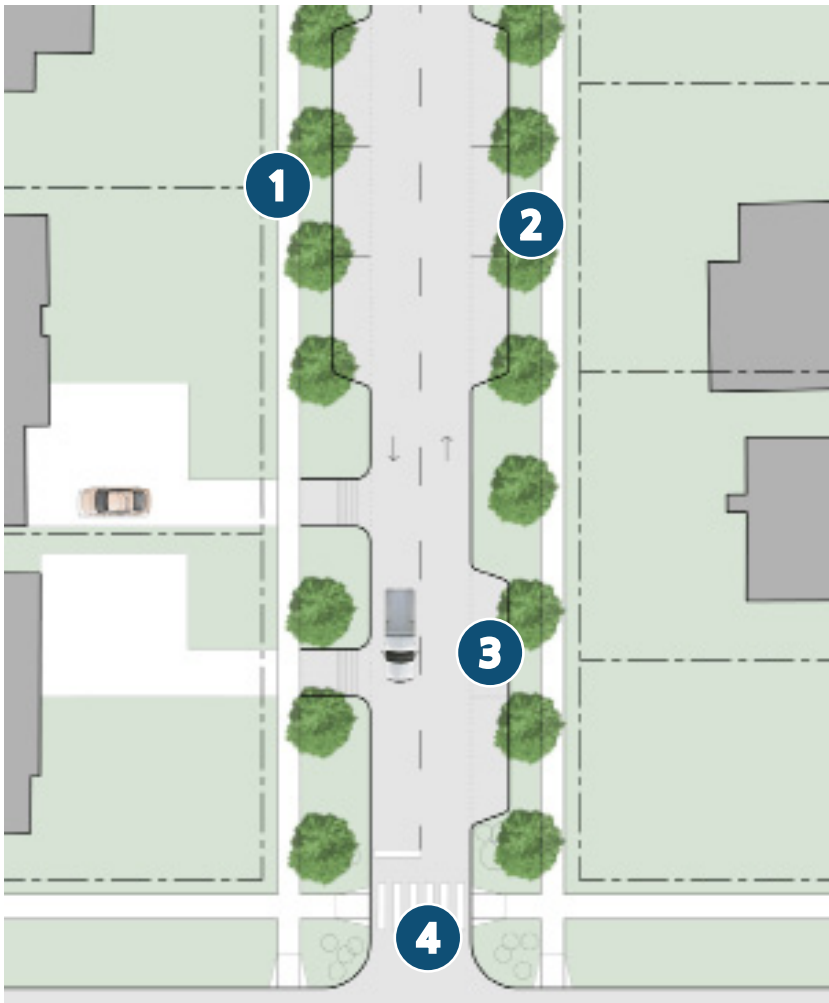
**Traffic buttons** are small scale neighbourhood roundabouts, that direct travel around a planted central island. This calms traffic and provides landscaping opportunities within the street right of way.



Boulevard planting enhances the public realm.



Street trees and sidewalks create a more pleasant environment for pedestrians and cyclists.



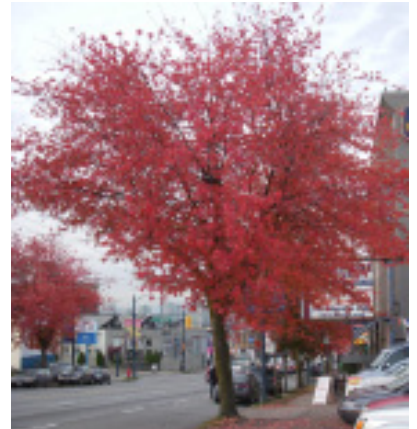
Local Street Plan

1



Sidewalks / boulevard standards.

2



Consider 'signature' trees to identify neighbourhood areas.

3



On street parking.

4

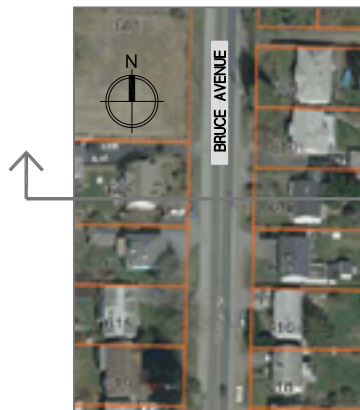
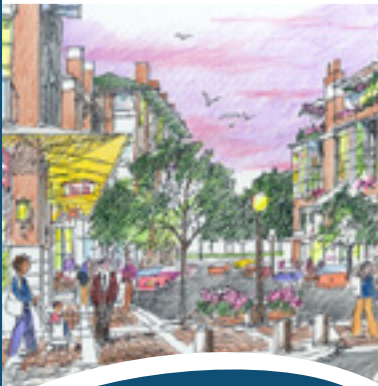


Crosswalks.

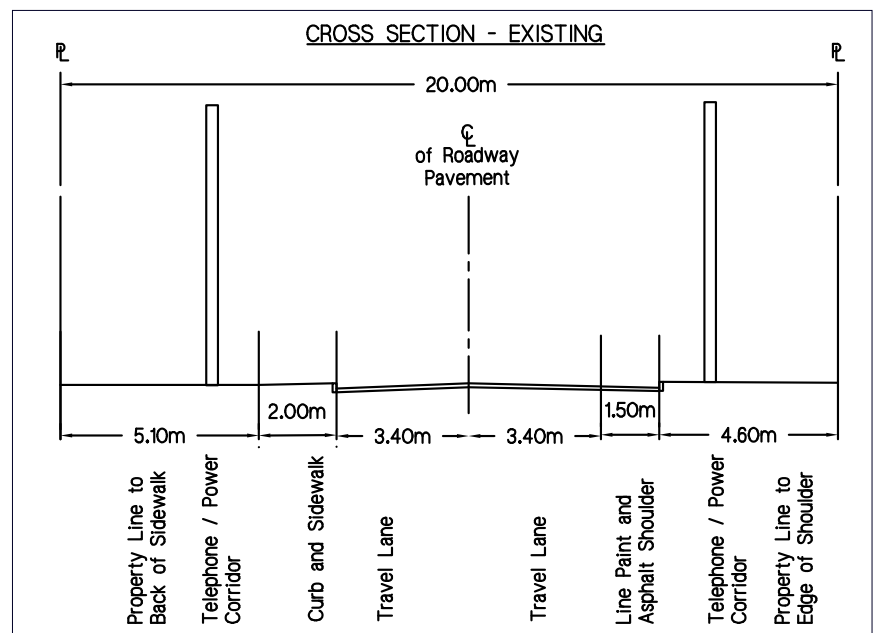
### 8.3 Bruce Avenue *Illustrated near Eighth Street*

#### Existing Street Conditions

Bruce Avenue is a north south road that runs the length of the neighbourhood. The street is flanked primarily by residential buildings, but also commercial areas at Fifth Street and Eighth Street. A number of residential and commercial driveways exit directly onto Bruce Avenue. The street has one traffic lane in each direction.



Existing Aerial View



Existing Cross Section

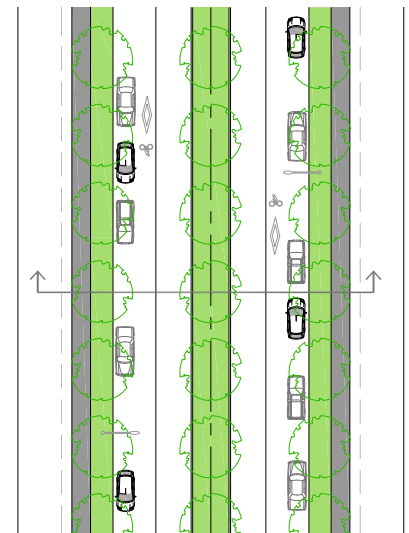
### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

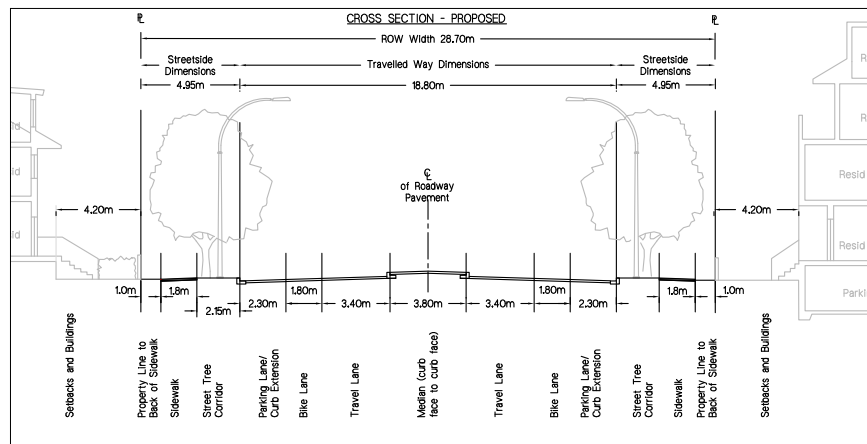
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Illustration



Proposed Street Plan

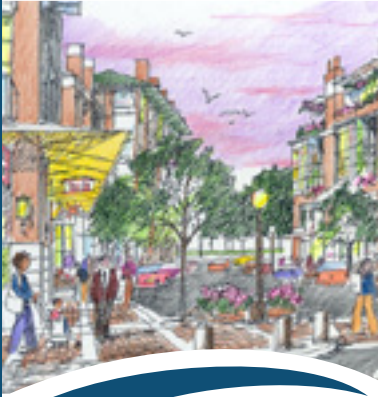


Proposed Cross Section

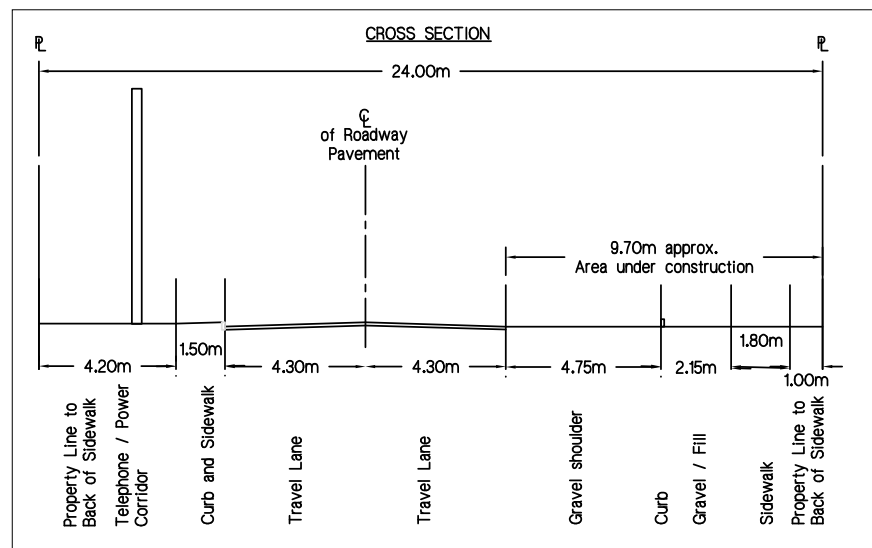
Bruce Avenue  
*Illustrated Near Fifth Street*

**Existing Street Conditions**

Bruce Avenue is a north south road that runs the length of the neighbourhood. The street is flanked primarily by residential buildings, but also commercial areas at Fifth Street and Eighth Street. A number of residential and commercial driveways exit directly onto Bruce Avenue. The street has one traffic lane in each direction.



Existing Aerial View



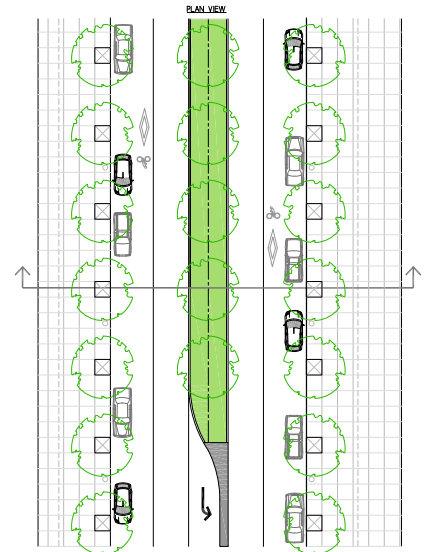
Existing Cross Section



### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

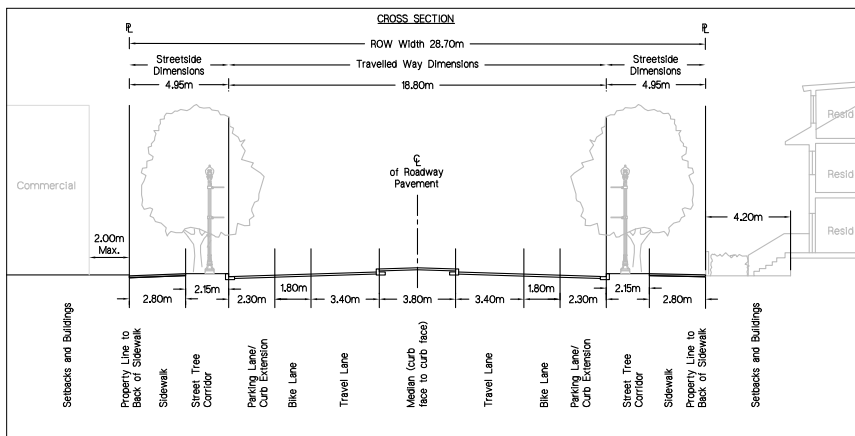
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration

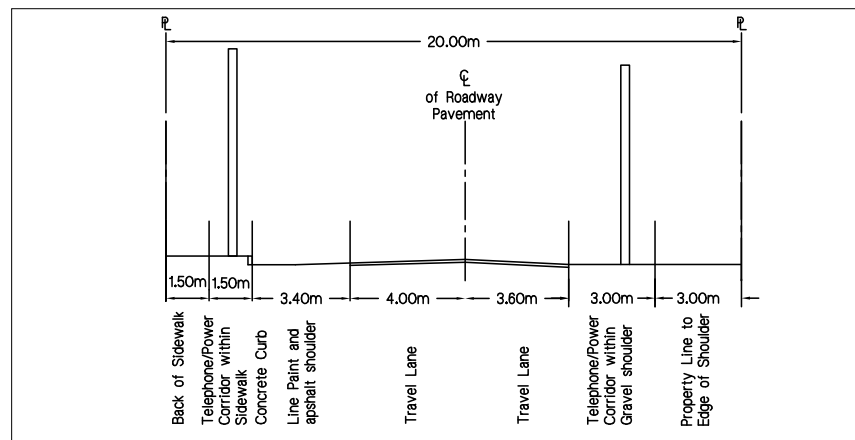
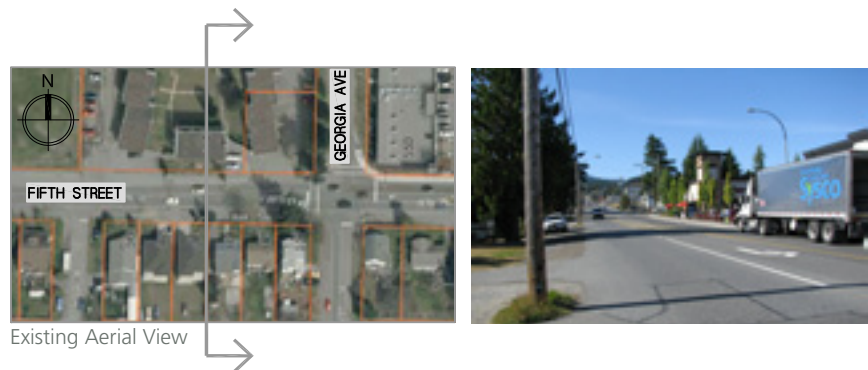
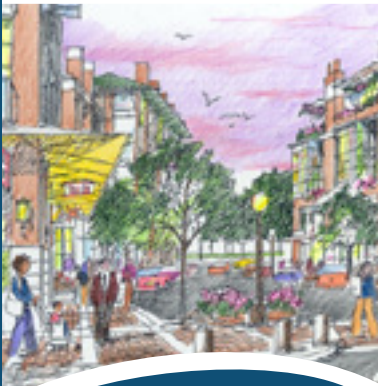


Proposed Cross Section

## 8.4 Fifth Street *Illustrated Near Georgia Avenue*

### Existing Street Conditions

Fifth Street is an east-west road that runs the length of the neighbourhood, connecting to Vancouver Island University, the Nanaimo Parkway, and the Island Highway. The street is flanked primarily by residential buildings, but also the commercial area at Bruce Avenue. A number of residential and commercial driveways exit directly on to Fifth Street. A signalized intersection currently exists at Fifth Street and Wakesiah Avenue. The street has one traffic lane in each direction, and some left turn bays are present.



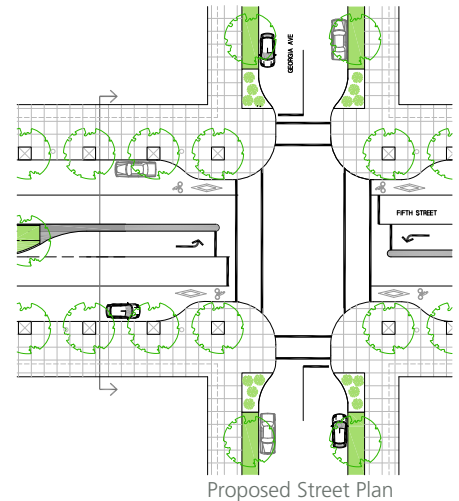
Existing Cross Section



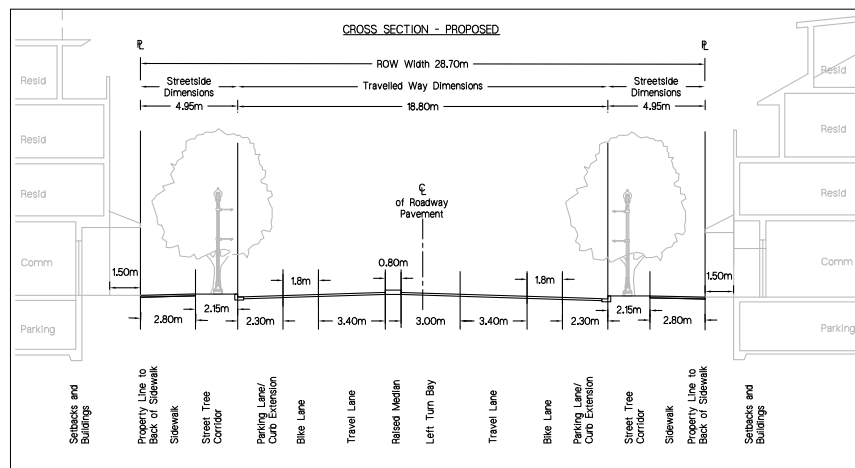
### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Illustration

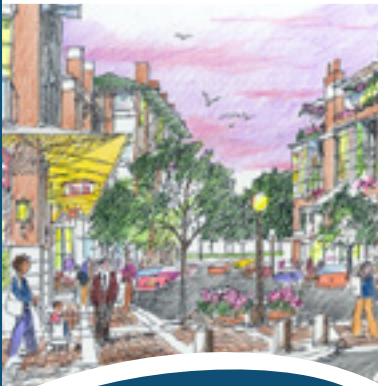


Proposed Cross Section

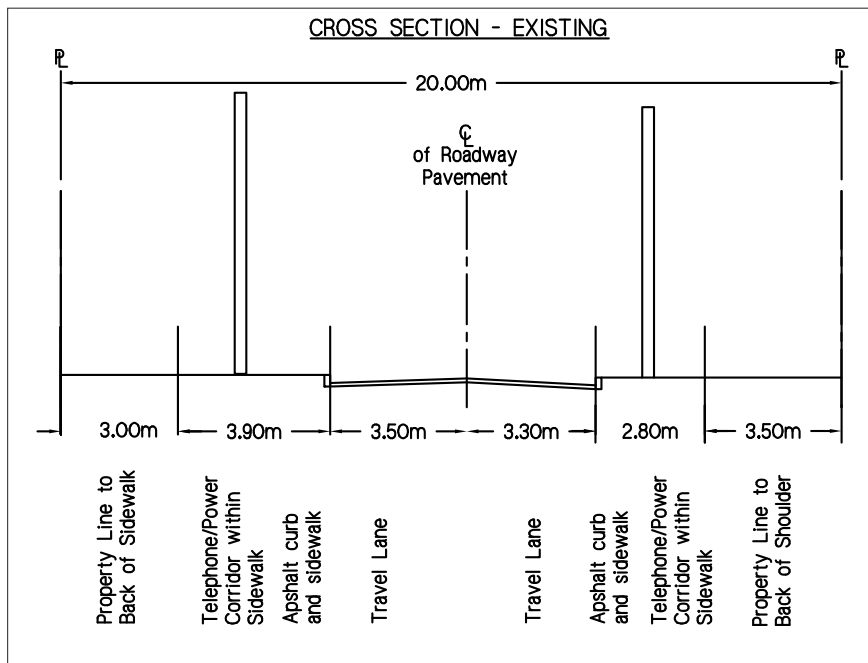
Fifth Street  
*Illustrated Near Bruce Avenue*

**Existing Street Conditions**

Fifth Street is an east-west road that runs the length of the neighbourhood, connecting to Vancouver Island University, the Nanaimo Parkway, and the Island Highway. The street is flanked primarily by residential buildings, but also the commercial area at Bruce Avenue. A number of residential and commercial driveways exit directly on to Fifth Street. A signalized intersection currently exists at Fifth Street and Wakesiah Avenue. The street has one traffic lane in each direction, and some left turn bays are present.



Existing Aerial View

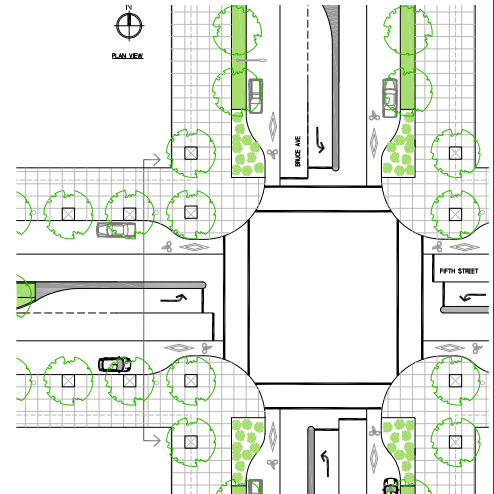


Existing Cross Section

### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

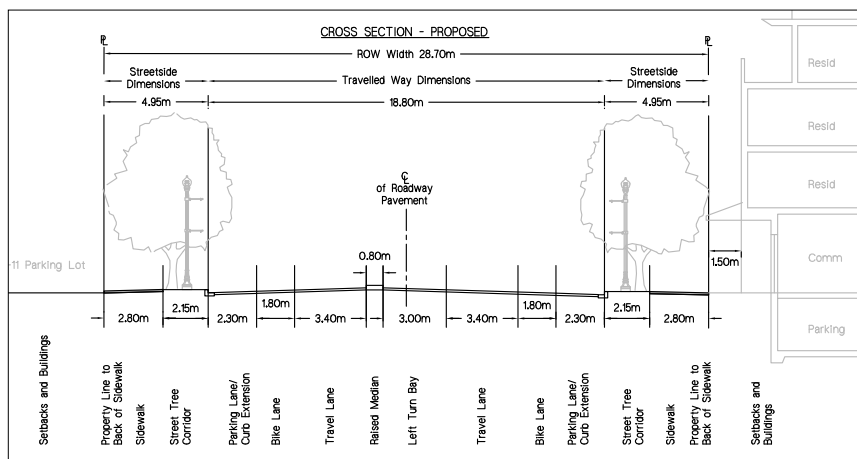
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration

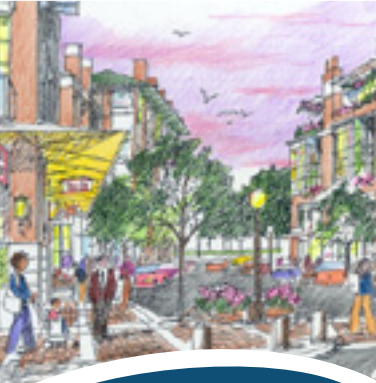


Proposed Cross Section

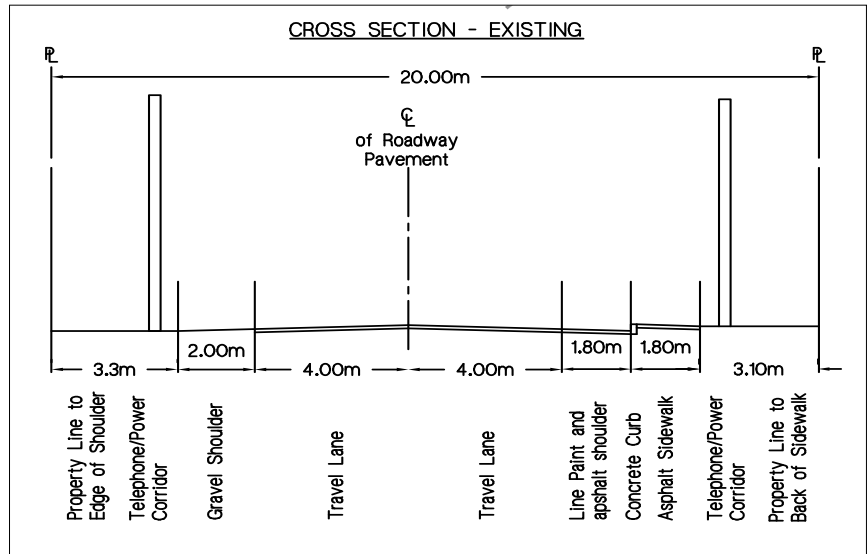
## 8.5 Fourth Street

### Existing Street Conditions

Fourth Street is an east-west road that runs the length of the neighbourhood, connecting the community to Vancouver Island University and downtown Nanaimo. The street is flanked primarily by residential buildings, but also a small commercial area at Bruce Avenue. A number of residential and commercial driveways exit directly on to Fifth Street. A signalized intersection currently exists at Fourth Street and Wakesiah Avenue. The street has one traffic lane in each direction, and a bicycle lane was recently installed.



Existing Aerial View

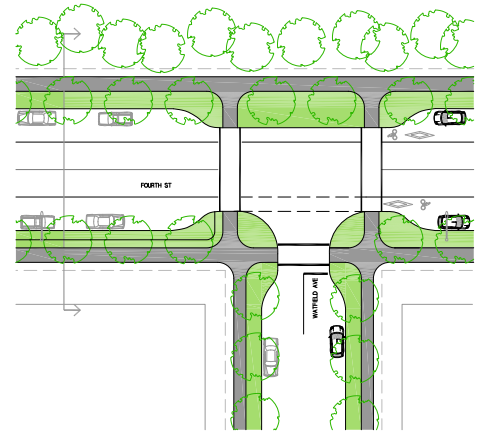


Existing Cross Section

### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

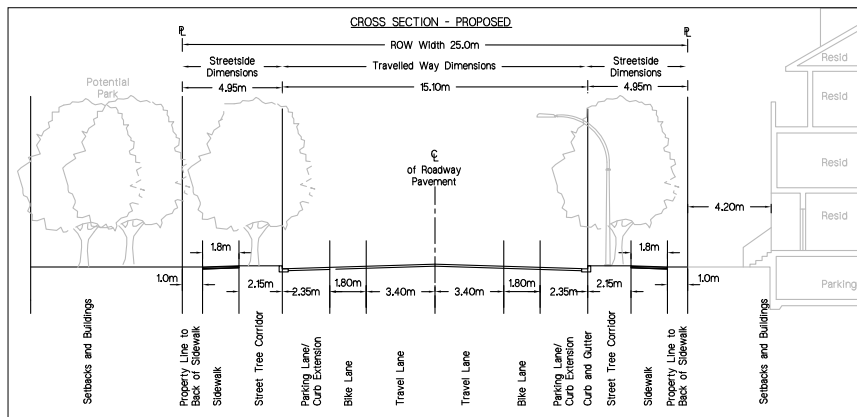
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration

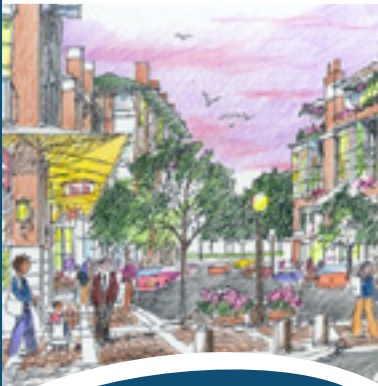


Proposed Cross Section

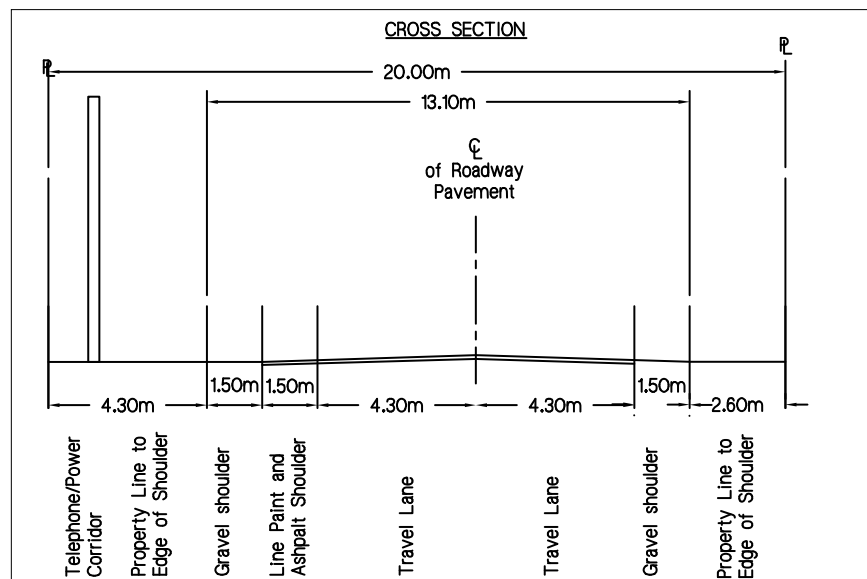
## 8.6 Tenth Street

### Existing Street Conditions

Tenth Street is an east-west road that runs along the southern border of the Neighbourhood Plan area, adjacent to the Nanaimo Parkway; and forms a connection from South Nanaimo to Bruce Avenue. The street is flanked by both residential buildings and some industrial operations. Driveway access is limited, but some residential and industrial driveways do connect directly to the street. The street has one traffic lane in each direction.



Existing Aerial View



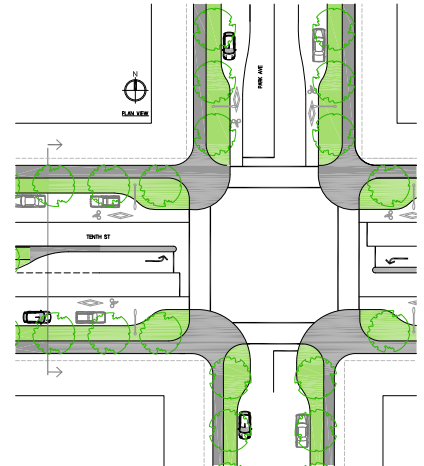
Existing Cross Section



### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

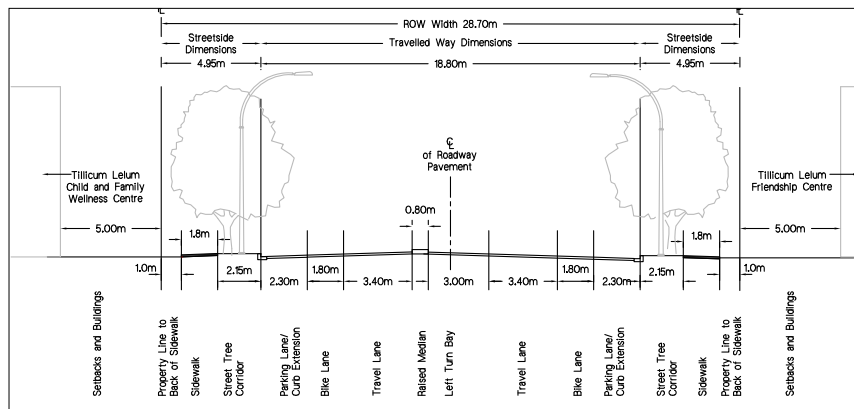
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration

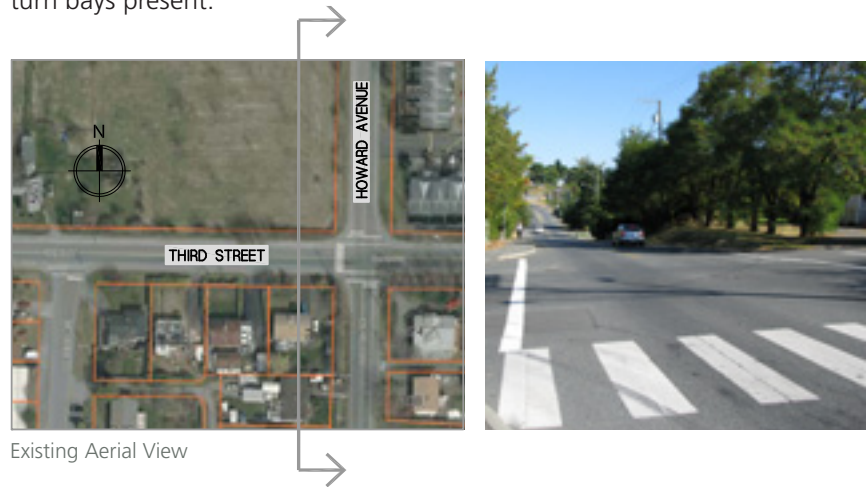
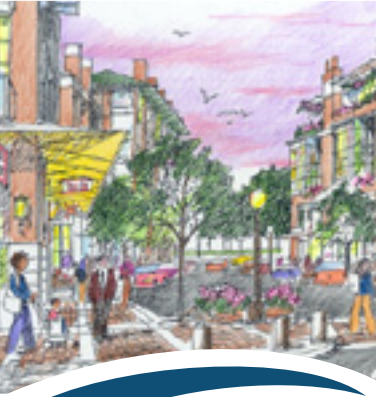


Proposed Cross Section

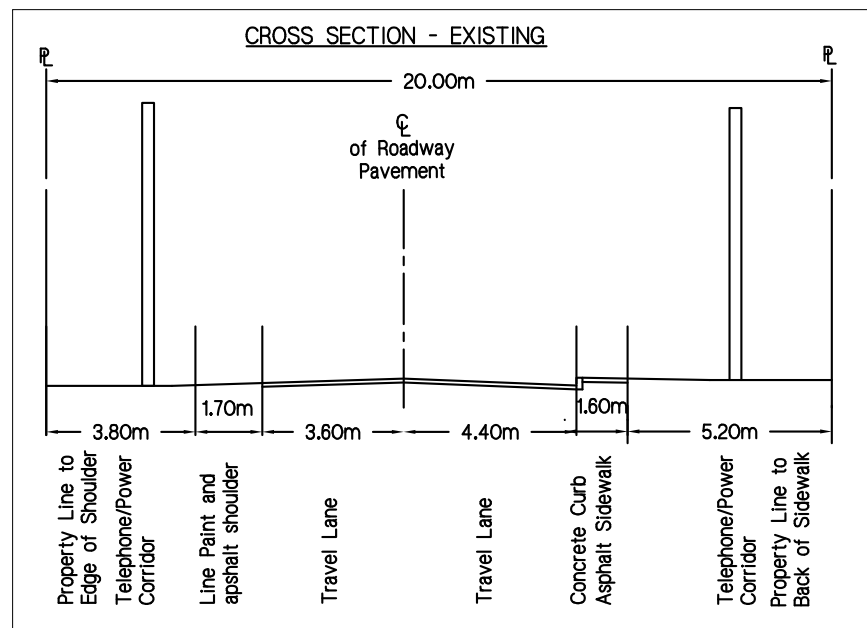
## 8.7 Third Street

### Existing Street Conditions

Third Street is an east-west road that runs along the northern portion of the Neighbourhood Plan area. Acting as the primary route from the Nanaimo Parkway through to downtown Nanaimo, it also connects the community with the Nanaimo Aquatic Centre, Nanaimo Ice Centre, and Serauxmen Sport Fields. The street is flanked predominantly by both single family and multiple family dwellings. Driveway access is limited, but some residential driveways do connect directly to the street. Two signalized intersections currently exist, one at Bruce Avenue and one at Wakesiah Avenue. The street has one traffic lane in each direction, with some left turn bays present.



Existing Aerial View

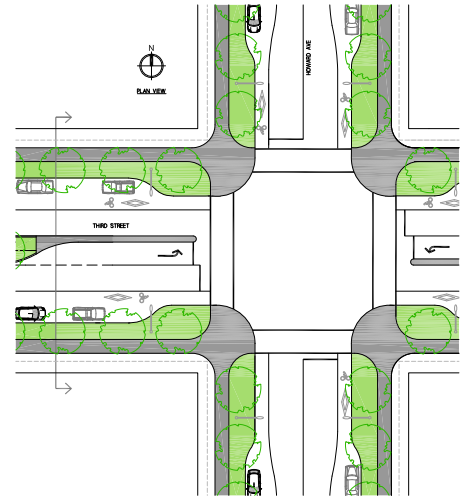


Existing Cross Section

### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

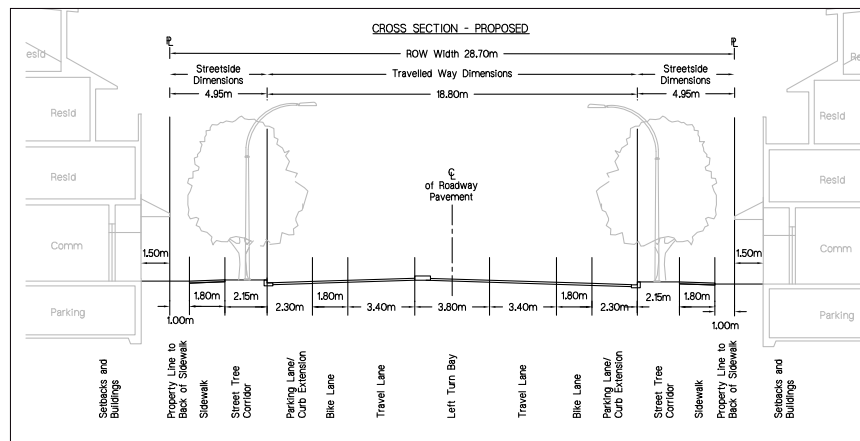
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration

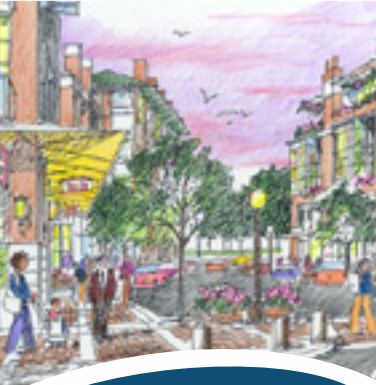


Proposed Cross Section

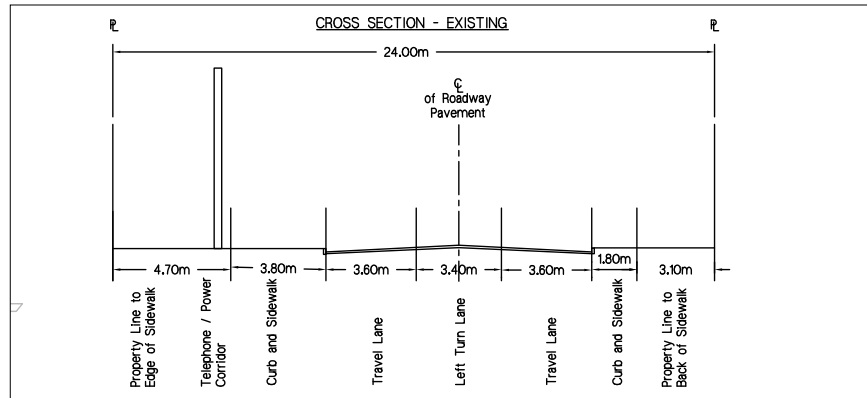
## 8.8 Wakesiah Avenue

### Existing Street Conditions

Wakesiah Avenue is a north-south road forming the northwest border of the Neighbourhood Plan; and acts as a primary collector road, connecting to Bowen Road, Vancouver Island University, schools and sports facilities. The street is flanked by a mix of single family, multiple family, commercial and institutional uses. A number of driveways exit directly on to the street. Signalized intersections are in place at Third Street, Fourth Street and Fifth Street, and a four way stop intersection is located at Sixth Street. The street has one traffic lane in each direction, with some left turn bays present.



Existing Aerial View



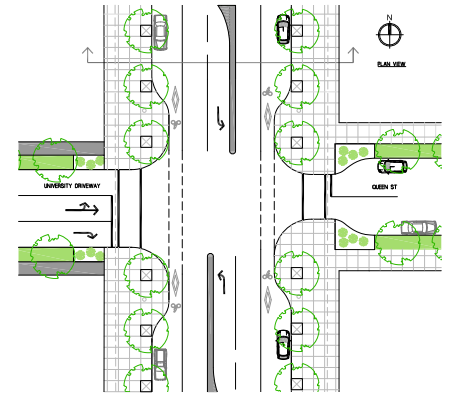
Existing Cross Section



### Proposed Conceptual Street Design

This conceptual street design is for reference, and will inform current engineering standards. All forthcoming road standards, dedications, and building setbacks must comply with the City of Nanaimo Manual of Engineering Standards and Specifications (MOESS) and Zoning Bylaw.

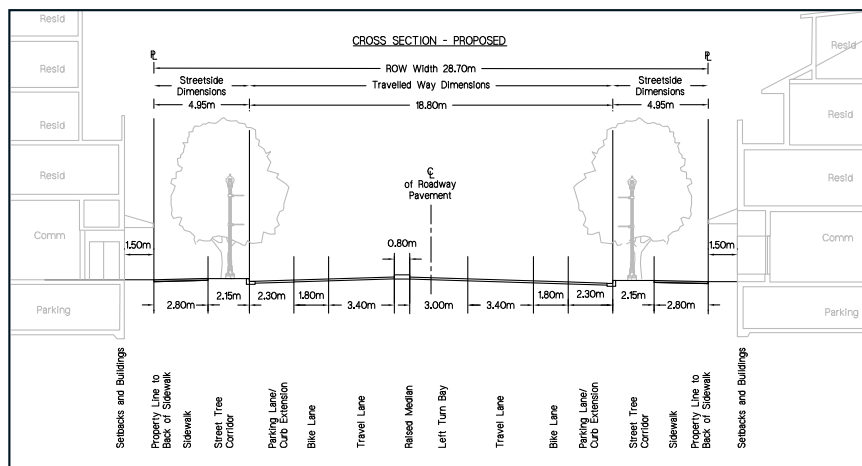
Street Design Guidelines are as laid out in Section 8.1 of the Harewood Neighbourhood Plan and the Harewood Neighbourhood Plan Transportation Plan and Policies Report dated 2013-MAY-30.



Proposed Street Plan



Proposed Street Illustration



Proposed Cross Section

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## **Part G** South Harewood Framework

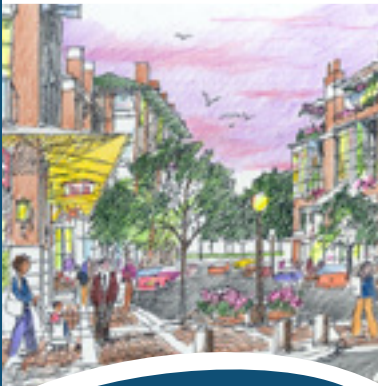
## 1 Introduction

The Framework for South Harewood covers the entire area of Harewood south of Seventh Street, and forms part of the Harewood Neighbourhood Plan.

South Harewood encompasses a substantial portion of the overall Harewood neighbourhood. It is also the part of Harewood that is least developed, and is comprised of large areas of remnant farmland and other undeveloped open space, forested areas, and creeks and riparian corridors. Several areas have recently been developed, typically at low residential densities with detached single family dwellings on fee simple lots. Much of the area is experiencing development pressure, and there are several active development sites. This Framework is intended to ensure that such development proceeds in a rational, efficient and consistent way that achieves the overall goals and objectives of the Harewood Neighbourhood Plan, protects the remaining natural assets, and creates a more sustainable urban community.

The following plan diagrams and text describe a number of key elements or systems of the Framework. These include the street network plan, the open space plan, and the proposed built form distribution plan. The last diagram in this series is a composite illustration of the overall Framework that combines all the plan elements in one single illustration.

A separate section of this Framework focuses on Design Guidelines for the South Harewood Village, which is intended to form the commercial core of South Harewood (see Part F, Section 4).

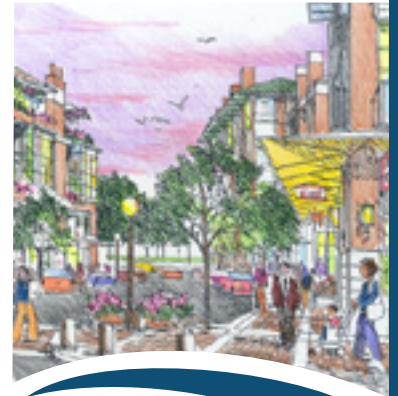


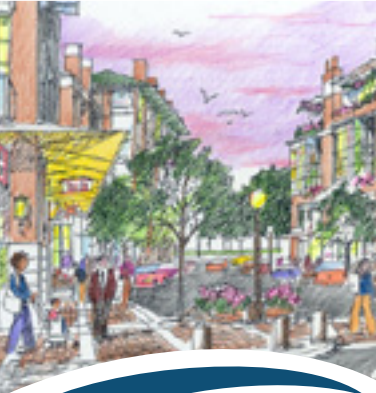


## 2 Street Network Plan

The current street network is incomplete and fragmented, with local streets developed on a project by project basis as subdivision occurs across the area. Many of these streets are only partially completed and often do not connect to the surrounding street network; which makes it difficult and inconvenient to get around the neighbourhood, especially on foot, forcing residents to use automobiles for even short local trips.

Figure 16 illustrates a proposed completed street network for the area, building on the existing street network. This includes all local streets and lanes. The proposed street network plan is an outcome of a workshop held with City staff and generally reflects the various road layouts that have either been approved, conditionally approved, or proposed for a number of subdivision applications in the area. A number of new streets, lanes and pedestrian connections are proposed that are not on any current subdivision application. These street layouts to be followed by the affected land owners, as and when subdivision or development proceeds for the affected areas.





The Street Network Plan provides a clear roadmap for creating a comprehensive, integrated, efficient and elegant local road network designed to help achieve multiple Neighbourhood Plan objectives, including:

- Create a more fine grained street network that supports alternative travel modes such as walking, cycling and transit, rather than a predominantly auto oriented system
- Support direct connectivity between the various parts of South Harewood, enhancing community interaction and accessibility, encouraging walking and cycling, and reducing auto dependence
- Protect valued natural features such as steep slopes, ridges, watercourses and riparian zones, water bodies, and an identified Environmentally Sensitive Area, by creating a 'modified' street grid that responds to and preserves these natural features
- Create efficient new development parcels that support a range of housing typologies such as cluster housing, townhousing, duplexes, multiple family dwellings, and narrow lot single family
- Create alternative route options and flexibility for traffic crossing through South Harewood
- Create a network of lanes that supports alternative forms of housing such as laneway housing, front to back duplexes, secondary suites, coach houses, and that better deals with parking, services, and so on.

The street network plan is a 'modified street grid'. It is based on a traditional rectilinear urban street grid that has been modified to take into account and avoid major natural features or obstacles such as creeks, ravines, ridges, steep slopes, forested areas, and environmentally sensitive areas. Streets generally connect to each other, and dead end streets (cul de sacs) are minimized, except where either already approved or unavoidable.

The street network plan includes extensions and connections to existing streets to optimize access and connections across South Harewood. Examples include the extension of Eighth Street west to Brookfield Drive and east to Douglas Avenue, which then connects south to Tenth Street; and the extension of Ninth Street west, connecting to Harewood Mines Road. These extended streets become more important routes and offer useful alternatives to the Bruce Avenue / Tenth Street arterial route. They help distribute and dilute traffic rather than concentrating it on one major arterial corridor. They offer alternative potential routes for cyclists and public transit.

The street grid includes a network of mid block lanes wherever practical. These lanes complement the streets and provide alternative access to sites for parking, loading, garbage collection, and so on. The lane network also supports alternative housing typologies such as laneway housing, front to back duplexes, secondary suites and coach houses. Lanes also allow all driveways to be at the rear of properties, thus eliminating numerous curb cuts across sidewalks on the streets, and permitting continuous street trees. These two measures help create a more walkable, friendly pedestrian environment, and support a more sustainable community.

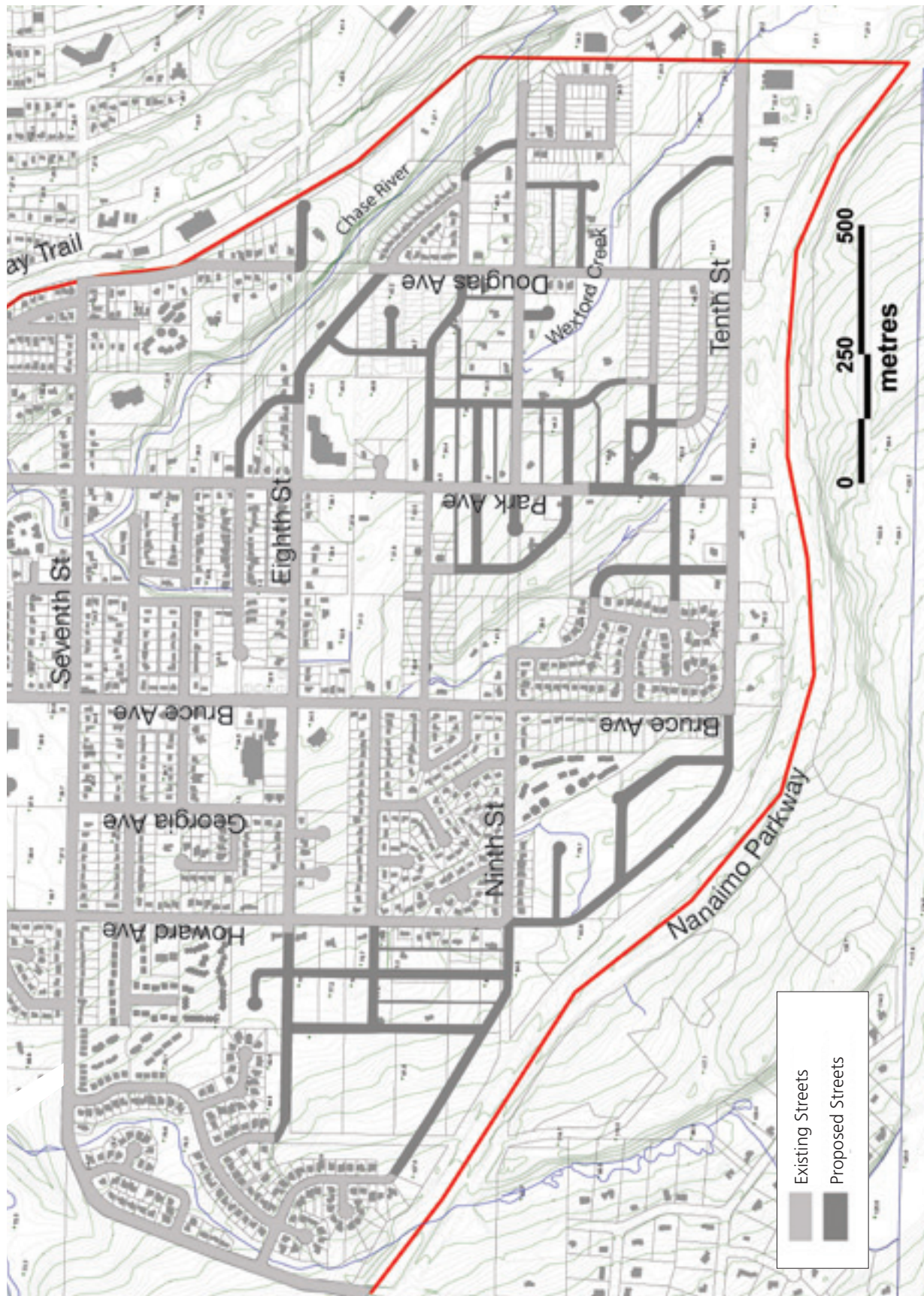
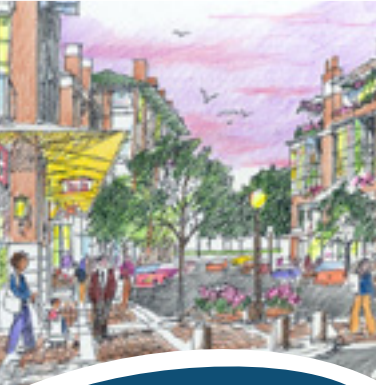


Figure 16 - South Harewood Street Network

### 3 Open Space Plan



The Open Space Plan builds on the existing public parks and open spaces in South Harewood, which are fragmented and often disconnected.

The Open Space Plan connects and links existing public parks and other open spaces, such as the school grounds. It also proposes to add additional open space.

A key intent is to create contiguous, uninterrupted green corridors that flank and protect the riparian corridors along all watercourses that flow through South Harewood. There are several such watercourses, most of which have been disrupted, buried, or diverted from their natural alignments due to urbanization and infrastructure development. There is also a natural low point in South Harewood where a water body has formed, and this area is proposed as a large protected green space (south of Eighth Street between Bruce Avenue and Park Avenue).

Much of the proposed open space is on privately owned lands. The expectation is that these lands can be secured as open space through the subdivision and / or development approvals process by means of dedications, easements, voluntary amenity contributions, or outright acquisition. In some areas, the open space may not need to be publicly acquired, provided the landowner agrees to avoid development in the identified portion.

The Open Space Plan also creates or extends a number of dedicated pedestrian routes through the South Harewood area, connecting to the various major public and community amenities such as the commercial centre, schools, parks, river trails, and so on. This network of public pathways and connections complements the street network and provides an alternative to automobile use. In some instances these pedestrian routes complete or extend existing routes. Pedestrian routes can be secured through the subdivision and development approvals process by means of dedications, easements, voluntary amenity contributions, or outright acquisition.

Another key intent of the Open Space Plan is to support the creation of a neighbourhood wide network of trails and pathways that connect all the watercourses and the major ridgeline that runs through the area.

The Open Space Plan also protects the Environmentally Sensitive Area (ESA) identified in the western portion of the area by creating a large open space that includes and surrounds this ESA.

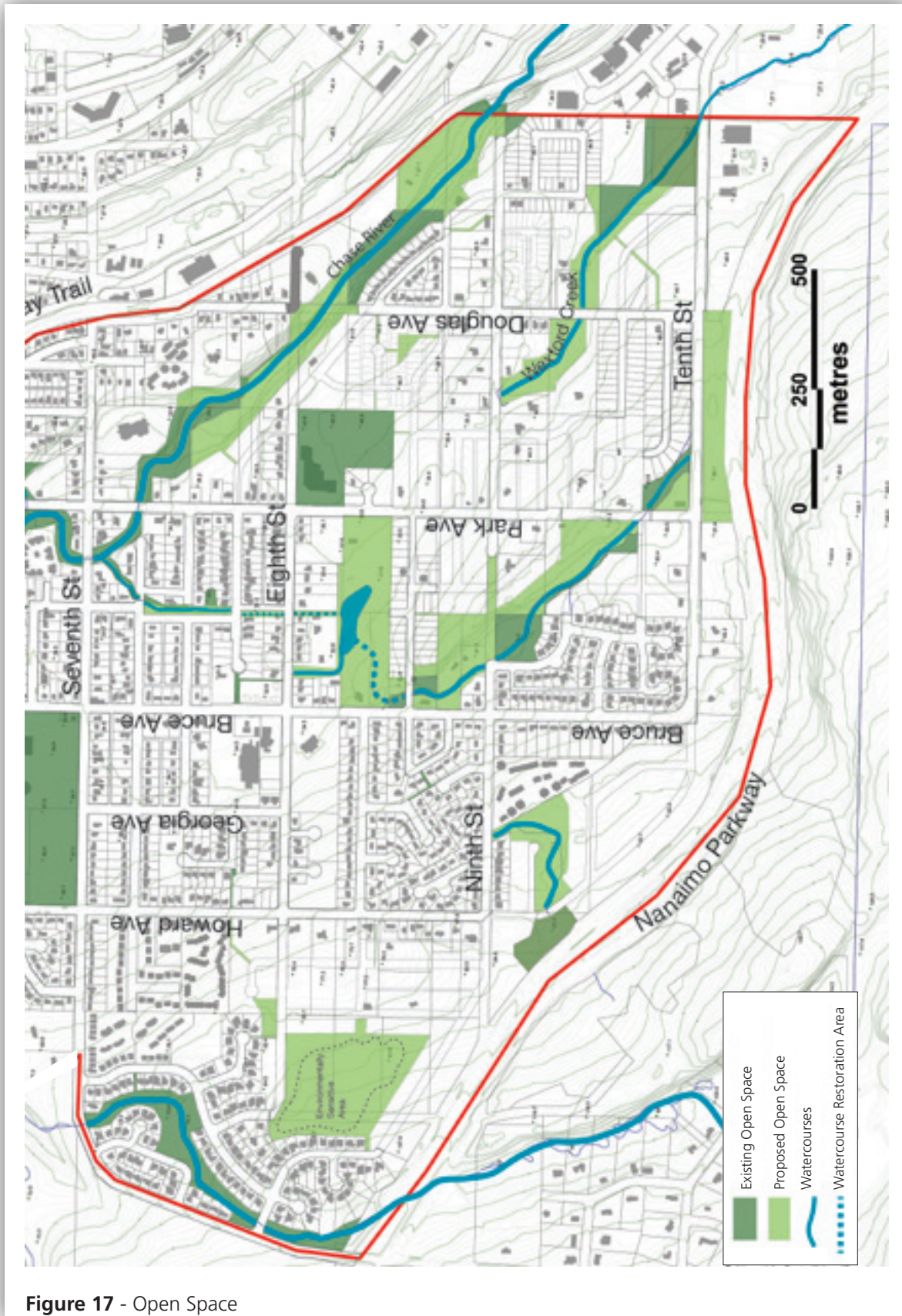
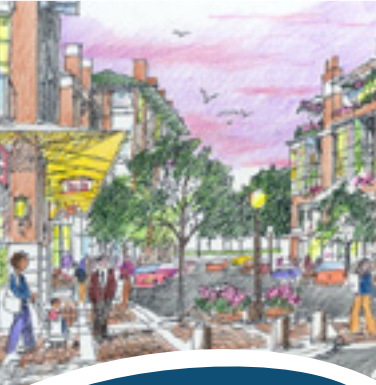


Figure 17 - Open Space

## 4 Built Form Distribution Plan



There is significant development pressure in the South Harewood area, and there are several active subdivision applications and development sites. At the same time, South Harewood is characterized by several remaining remnant open farmland parcels, forested areas and other undeveloped areas, which are highly valued by the community.

This Built Form Distribution is intended to ensure that future development proceeds in a rational, efficient and consistent way that protects the remaining natural assets, saves some of the remnant farmland for future urban agriculture or recreation use, and creates a more sustainable urban community by promoting more compact, less wasteful forms of development. A key strategy for achieving this objective is to promote and encourage forms of residential development that are more compact, denser, use less land, and are less auto dependent.

The Built Form Distribution illustrates a range of land uses. These include Single Family Residential, Multiple Family Residential, Commercial and Industrial.

### ***Single Family Residential***

The areas illustrated in the Land Use Plan as Single Family residential would permit both traditional (larger lot) Single Family Residential and Narrow Lot Single Family Residential.

#### *Traditional (larger lot) Single Family Residential*

- Refers to typical larger lot singlefamily residential development
- Each unit has its own separate lot with street frontage
- minimum frontage width is typically 10 metres (33 feet) or 15 metres (50 feet), or wider
- Parking is provided on site in attached garage, either via the street or from a rear lane (where present)
- Typically two storeys
- Tenureship typically provided through fee simple ownership
- Lowest density form of residential development

#### *Narrow Lot Single Family*

- Similar to traditional (large lot) Single Family Residential, but with narrower frontage.
- Minimum frontage width is typically 7.6 metres (25 feet), and can be even narrower where local zoning bylaw permits
- Typically two or three storeys
- Tenureship typically provided through fee simple ownership
- Second lowest density form of residential development, after typical larger lot single family development

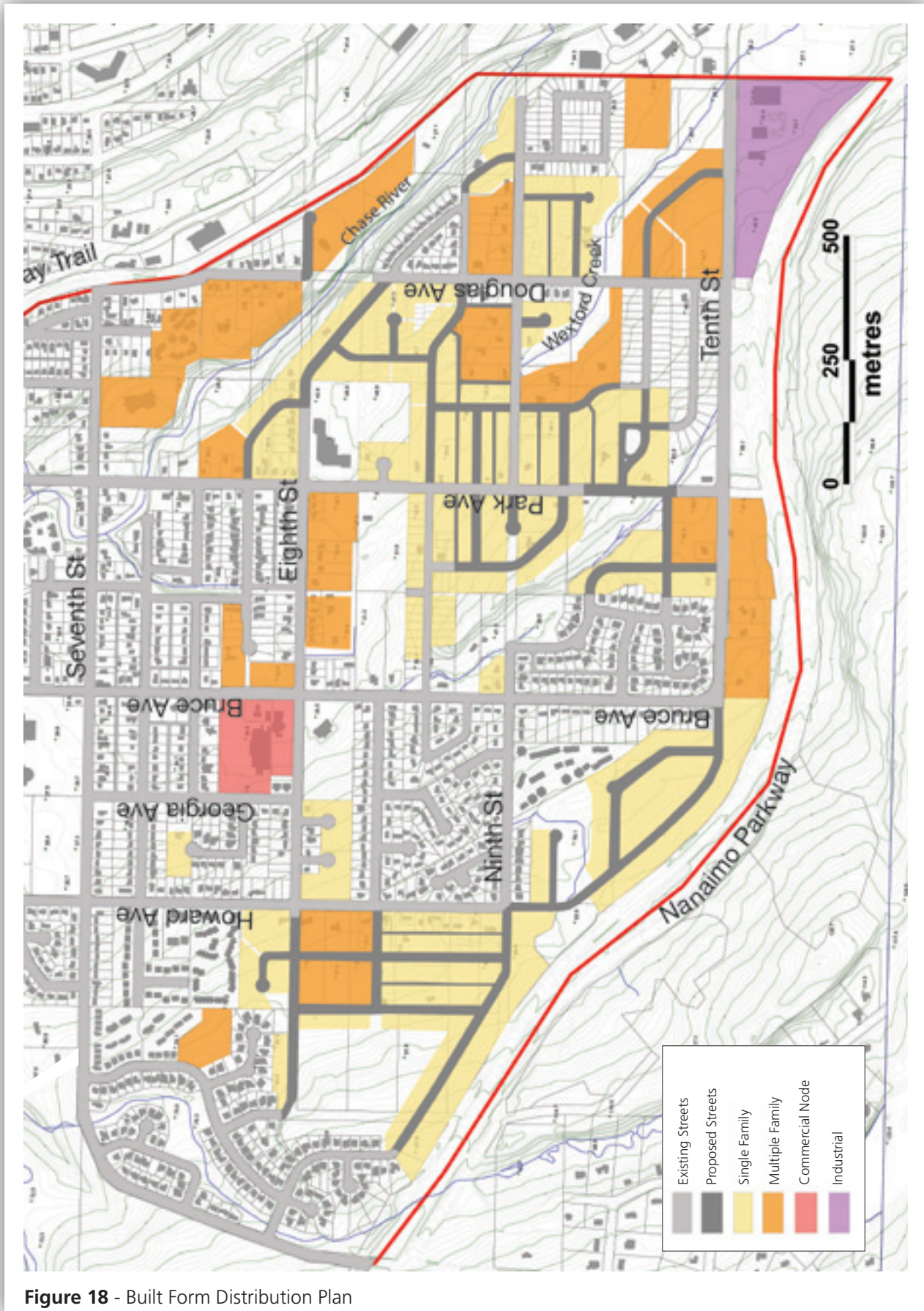
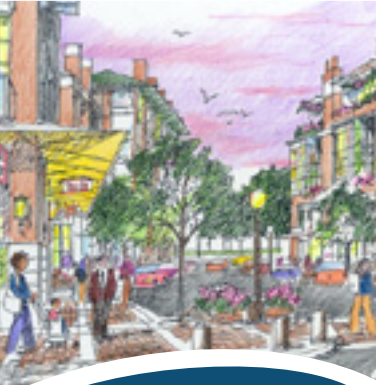


Figure 18 - Built Form Distribution Plan



### ***Multiple Family Residential***

A number of sites have been identified on the Built Form Distribution as potential future Multiple Family residential. These sites are typically larger and have (or will have, with development of the proposed street network) good access. In order to promote forms of residential development that are more compact, denser, use less land, and are less auto dependent, a range of Multiple Family housing typologies are encouraged on these sites, including:

#### *Cluster Housing*

- Multiple housing units developed in clusters with common driveways, parking areas and, in some cases, shared open space
- Provides an alternative to the typical dead end (cul de sac) street single family housing subdivision plan
- Allows development of a site to its full permitted density, while preserving a portion of the site as open space
- May be incentivised by conditionally permitting increased density, in return for preserving a larger portion of the site

#### *Duplex (Semi Detached) Housing*

- Two units attached to each other
- Can be attached side by side or front and back
- Units are typically unattached on two or three sides
- Typically two or three storeys
- Parking is typically in a shared parking space or side by side garages
- Can be combined into triplex or fourplex configurations, resulting in further land use economies
- Achieves higher densities and uses less land than traditional single family housing or narrow lot single family housing

#### *Townhousing*

- Attached ground oriented housing units
- Units typically attached side by side
- Units accessed directly from grade through front door to street or courtyard
- Typically two or three storeys
- Each unit has its own parking garage
- Can also include stacked townhouses, where the ground floor is one unit and the upper floor(s) is a separate unit, also accessed from grade
- Tenureship may be provided through strata ownership or fee simple ownership where local zoning bylaw permits
- Achieves higher densities and uses less land than semi detached duplex housing



*Multiple Family Dwellings*

- Multiple units in a multi storey apartment building
- Common principal entry, elevator, and corridor access to units
- Typically two to four storeys, wood frame construction
- Generally requires underground parking, or common surface parking area
- Tenureship is typically provided through strata ownership or rental
- Achieves higher densities than Duplex or Townhousing typologies

**Commercial**

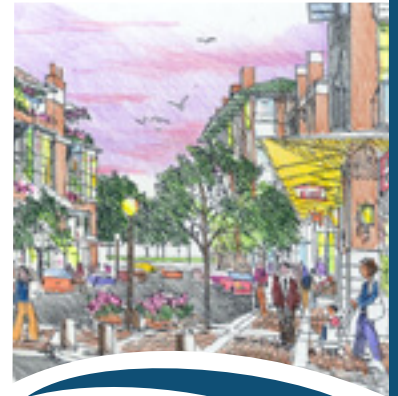
The Built Form Distribution identifies an area as Commercial. This is the South Harewood Village, which is intended to provide local retail and services to South Harewood. This neighbourhood commercial centre is defined as the block bounded by Georgia Avenue, Eighth Street and Bruce Avenue, south of the existing houses facing Deering Street; and excludes the residential development at the corner of Georgia Avenue and Eighth Street. This site is proposed for redevelopment.

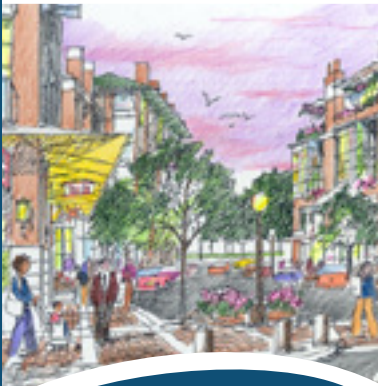
Design Guidelines for the site are found in Part F, Section 4 of the Harewood Neighbourhood Plan. The intent of the guidelines is to encourage the development of a neighbourhood commercial centre that provides goods and services to the South Harewood area, is carefully planned as an urban centre, is compatible in form and scale with surrounding development, and optimizes pedestrian usage and amenity.

**Industrial**

The Built Form Distribution identifies an area as Industrial. This is defined as the area south of Tenth Street and east of Douglas Avenue.

The existing industrial area is proposed to remain in light industrial use. Any change in proposed industrial uses will need to be reviewed for compatibility with residential development on the north side of Tenth Street.





## 5 Comprehensive Development Plan

This Comprehensive Development Plan illustrates in a single integrated plan the various Framework elements overlaid, including built form distribution, street and pedestrian network and open space network.

The Plan illustrates how these elements integrate to form a coherent, rational, efficient and more complete neighbourhood that achieves the overall goals and objectives of the Harewood Neighbourhood Plan, protects the remaining natural assets, preserves some of the remnant farmland, and creates a more sustainable urban community.

Key features of the Comprehensive Development Plan include a finer grained, interconnected street grid that is modified to avoid key natural features and environmentally sensitive areas, coupled with an open space network that preserves and connects these areas as well as some remnant farmland for future urban agriculture or recreation use; and a range of land uses that will create a more sustainable urban community by promoting more compact, less wasteful and less auto dependent forms of development.

The Comprehensive Development Plan is a roadmap for future private development and public investments in South Harewood. It serves as a guide both at the macro neighbourhood level, and at the micro site specific level. It should be referred to for all future decisions on land use and development approvals, street and other public infrastructure capital projects, parks and open space acquisitions, and environmental priorities. The South Harewood Comprehensive Development Plan supports the overall goals and policies of the Harewood Neighbourhood Plan.

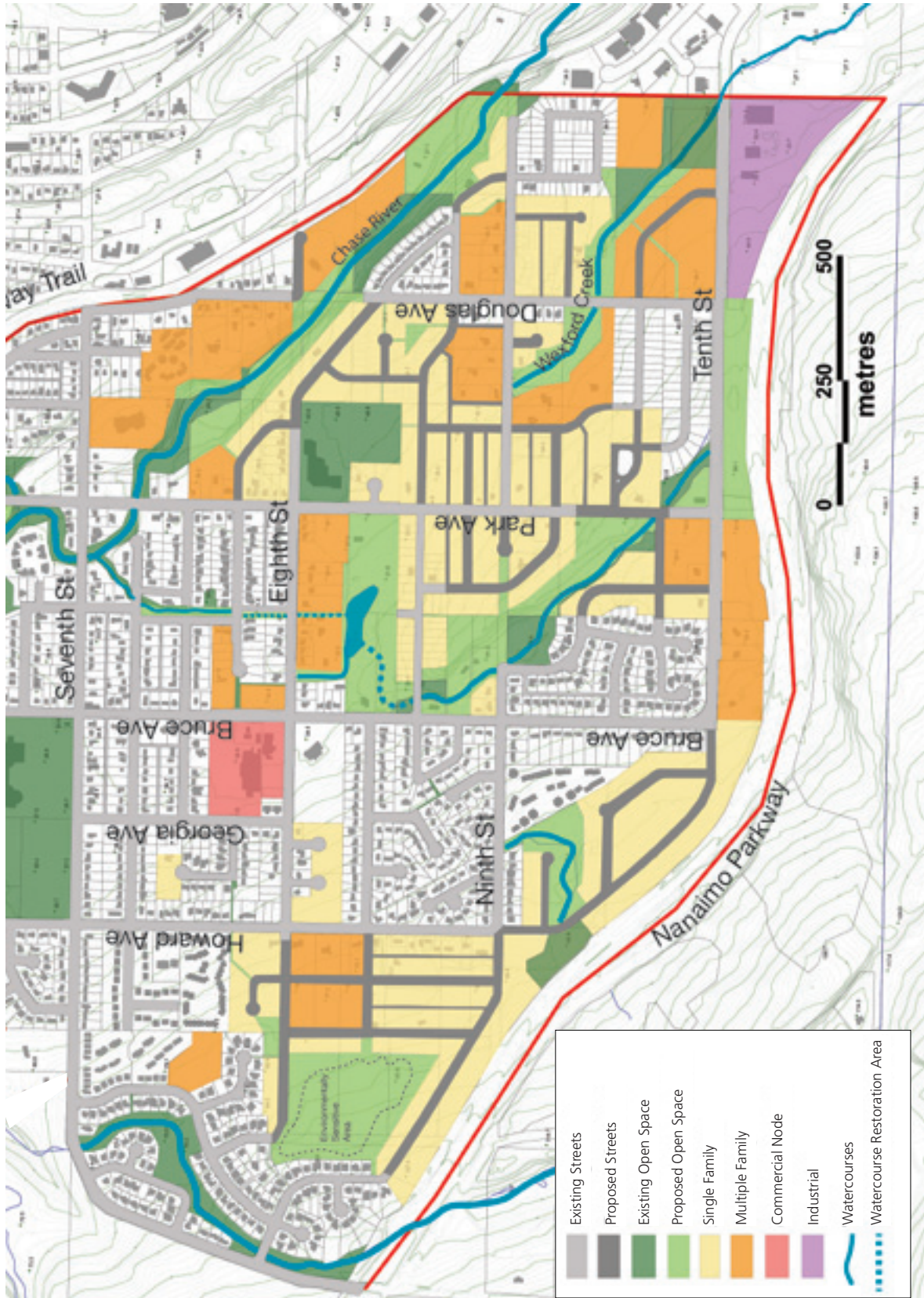
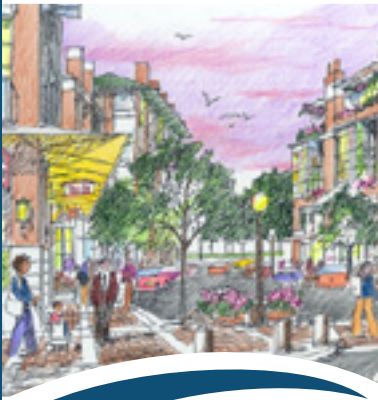


Figure 19 - South Harewood Comprehensive Development Plan

## 6 Preserving Open Space Through Alternative Housing Typologies



A key objective of the South Harewood Framework is to preserve natural features such as steep slopes, ridges, watercourses, riparian zones, habitat corridors, water bodies, and some of the remnant farmland, while encouraging forms of development that help create a more sustainable urban community. An important strategy for achieving this objective is to promote and encourage forms of residential development that are more compact, denser, and use less land than traditional single family detached housing.

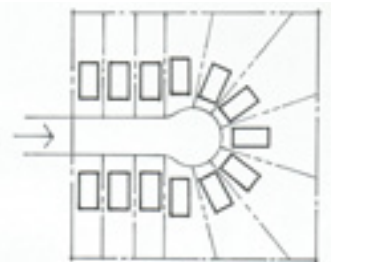
A key housing typology that can help achieve this is 'Cluster Housing'. Cluster Housing refers to ground oriented housing units that are developed in clusters, typically with common driveways, parking areas and, in some cases, shared open space. Cluster Housing can take the form of attached or semi detached townhouses or duplexes.

Cluster Housing provides a land efficient alternative to dead end streets (cul de sac) single family housing subdivision plans, as it enables development of a given site to its full permitted density (units per hectare), while preserving a portion of the site as open space.

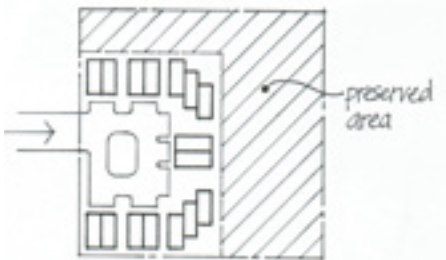
This form of housing may also be incentivised by conditionally permitting increased density (i.e. more units) than would be permitted in a typical single family housing layout, in return for preserving a larger portion of the site.

The adjacent diagrams compare alternative conceptual layouts for the same generic development site. It illustrates the typical single family housing layout on a cul de sac, versus a conceptual Cluster Housing layout that uses only a portion of the site and preserves the remainder of the site. The single family housing conceptual layout shows 13 detached housing units arranged around a typical cul de sac, while the Cluster Housing conceptual layout achieves higher density with 16 semi detached housing units. This is achieved by clustering the housing more tightly around a central driveway, parking area and common open space. The increase in density would help reduce any sale price differential that may exist between the two different housing types (single family detached versus semi detached duplex) and add to the developer's profit, thus encouraging this form of development while preserving more open space.

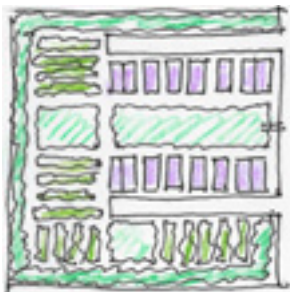
*Note: These diagrams are for illustrative purposes only and do not represent an actual site or development proposal. It is provided to illustrate a site planning principle.*



Typical single family housing layout on cul de sac (~13 units).  
Not to scale



Cluster housing layout on cul de sac (~16 units).  
Not to scale



Cluster housing layout incorporating urban agriculture.  
Not to scale



## **Part H** Implementation Strategy

- 1 Development Amenities
- 2 Monitoring the Neighbourhood Plan
- 3 Implementing the Neighbourhood Plan
- 4 Implementation Strategy

## 1 Development Amenities

The Neighbourhood Plan identifies a range of items to be considered in the review of development applications in the Harewood Neighbourhood Plan area. These items are typically considered as amenities that may be provided as part of rezoning and comprehensive development approval. Identification of these amenities is particularly useful in establishing the priorities for the neighbourhood in conjunction with proposed developments.

### Policies

In recognition of the need for new development within the Harewood Neighbourhood Plan area to contribute to the amenities and services from which they also benefit, development proposals that involve rezoning will be requested to include some public amenity items as part of the completed project. The following amenities are provided for consideration, in order of priority, as development occurs.

- Bike paths and / or lanes, trail installations and improvements, and additional pedestrian connections
- Community urban agriculture gardens and open space
- Public meeting space, which could include public plazas, meeting rooms, stand alone community centres, playing fields
- Green boulevard features, including street trees and stormwater strategies
- Affordable housing
- Public art
- On site heritage building conservation
- Environmental enhancement and sustainability
- Green design strategies, which could include green roofs, on site renewable energy
- Underground parking
- View protection
- Traffic calming measures, which could include pedestrian crosswalks, roundabouts



Site specific conditions will also determine what amenities are indicated for consideration in a project. Criteria for consideration of possible amenities may include:

- Site characteristics: natural features that are environmentally, historically or archaeologically sensitive and needing protection; viewscales; outdoor recreation opportunities
- Other features identified in the Neighbourhood Plan (for example, trails, public plazas)
- Needs of the immediate neighbourhood
- Size of proposed development
- Nature of proposed development
- Projected population in site



## 2 Monitoring the Neighbourhood Plan

Throughout the implementation process, it is important that the City maintains open communication with neighbourhood residents on the progress of the Neighbourhood Plan. Upon adoption, the Neighbourhood Plan will be monitored by the City, with the progress of the Neighbourhood Plan reviewed annually with the neighbourhood association. The main purpose of monitoring the Neighbourhood Plan is to:

- Ensure that action items are moving forward as anticipated
- Assess the impacts of current projects
- Ensure that action items are still consistent with community aspirations
- Ensure the Neighbourhood Plan remains valid by carrying out an annual review to identify accomplishments and actions still needed or not previously identified

### 3 Implementing the Neighbourhood Plan



The Harewood Neighbourhood Plan will be implemented through a range of measures, from bylaw amendments to direct involvement and communication with the City, residents, landowners and other stakeholders. Certain actions will be achievable in the short term while others may require a longer time period. Other specific action items are ongoing, with no set completion date.

All proposed changes to this Neighbourhood Plan must follow the amendment procedures laid out in the *Local Government Act* and the Official Community Plan. In addition, any proposed changes to the Neighbourhood Plan will be referred to the Nanaimo Advisory Planning Committee and advertised as required by the *Local Government Act* to provide residents with the opportunity to comment on issues.

The Implementation Strategy for this Neighbourhood Plan is presented in the following pages. The Strategy identifies actions, timing and responsibilities of the City, other government agencies, organizations and community groups. Neighbourhood Plan implementation is the responsibility of both the City of Nanaimo and its partners, recognizing that certain issues require the involvement of residents, business interests, and / or other stakeholders.

The Strategy, as outlined in Subsection 4 (Implementation Strategy), sets a series of five time frames for the action items as follows:

- **Immediate** to be initiated within one year of adoption of the Neighbourhood Plan;
- **Short Term** to be completed within 5 years;
- **Medium Term** to be completed within 5 to 10 years;
- **Long Term** to be completed over 10 to 20 years; and
- **Ongoing** to be initiated in the short term with no planned date of completion.



## 4 Implementation Strategy

Implementation Action	Timing	Plan Reference	Completed
<b>Land Use and Development</b>			
<i>Amend Map 1 – Future Land Use of the OCP to be consistent with Map 1 – Neighbourhood Land Use of the Harewood Neighbourhood Plan</i>	Immediate	Section E 2.1	
<i>Amend the Development Permit Area 9 guidelines contained in Section 7.4 of the OCP to reference the Harewood Urban Design Guidelines.</i>	Immediate	Part F Part G	
<i>Explore creation of a zone classification to allow cluster housing and protect open spaces.</i>	Short Term	Section E 2.16	
<b>Social Enrichment and Culture</b>			
<i>Work with the neighbourhood association to identify the amenities required and suitable locations for siting of a community centre(s), including a review of those sites identified in Section 3.2.</i>	Short Term	Section E 3.1 Section E 3.2	
<i>Review the Nanaimo Cultural Strategy and the Community Plan for Public Art to address public art and culture specific to the Neighbourhood Plan area.</i>	Medium Term	Section E 3.4	
<i>Evaluate and add neighbourhood buildings, sites and landscape, as set out in Figure 3, to the City's Community Heritage Register, subject to owner support.</i>	Medium Term	Section E 3.5 Section E 3.6	
<i>Work with the neighbourhood association to design and implement a program that establishes criteria for the placement of social, health and support services within the neighbourhood.</i>	Short Term	Section E 3.9	
<i>Explore opportunities to promote urban agriculture within the neighbourhood through creation of the Nanaimo Food Strategy.</i>	Medium Term	Section E 3.14	

Implementation Action	Timing	Plan Reference	Completed
<i>Set up a formal liaison between the neighbourhood association, the City and RCMP for the purposes of communication and partnership on providing neighbourhood stability and continued security</i>	Ongoing	Section E.23	
<i>Work with the neighbourhood association and community schools to identify recreational opportunities and other activities in the neighbourhood for all age ranges, ensuring accessibility and undertaking trail and roadway improvements.</i>	Ongoing	Section E 3.27 Section E 3.32	
<i>Meet periodically with the neighbourhood association to evaluate progress of the Neighbourhood Plan.</i>	Ongoing	Section E. 3.33	
<i>Continue working with the neighbourhood associations to review development proposals, and support and promote upcoming local events.</i>	Ongoing	Section E 3.34 Section E 3.35	
<i>Review registration of the neighbourhood association, as laid out under the Society Act; and continue efforts to build stability and organizational capacity.</i>	Ongoing	Section E 3.36	
<b>Economic Development</b>			
<i>Review the feasibility of establishing a Development Incentive Program that encourages mixed use commercial / residential development on lands designated Village Corridor within the Neighbourhood Plan. The program may include the following incentives:</i> <ul style="list-style-type: none"> <li>• Residential density bonusing</li> <li>• Rezoning of select lands</li> </ul>	Short Term	Section E 4.1	
<i>Develop a promotional strategy to generate economic activity within the Harewood Village area, and to rebrand Harewood as a vibrant, welcoming and unique neighbourhood. This would include development of an information brochure to promote Harewood.</i>	Medium Term	Section E 4.2 Section E 4.9 Section E 4.10	

Implementation Action	Timing	Plan Reference	Completed
<i>Continue discussions between Vancouver Island University and neighbourhood residents to improve communications, and develop programs and services within the neighbourhood.</i>	Ongoing	Section E 4.8	
<b>Environmental Protection and Enhancement</b>			
<i>Develop a strategy to provide for additional land dedication along watercourses for installation of community trails.</i>	Short Term	Section E 5.1	
<i>Pursue opportunities to daylight the watercourse located along Stirling Avenue, as identified in Figure 10.</i>	Long Term	Section E 5.3	
<i>Review and consider amendments to current City regulations as they relate to alternative rainwater management systems.</i>	Medium Term	Section E 5.4	
<i>Pursue an interpretive signage program to highlight rainwater management systems and environmental sensitivities.</i>	Medium Term	Section E 5.5	
<i>Work with the community and external agencies to name the 'unnamed creek' that forms part of the Chase River system, as shown on Figure 9.</i>	Short Term	Section E 5.5	
<i>Partner with community groups to undertake environmental activities such as invasive species management and revegetation, and street tree planting.</i>	Ongoing	Section E 5.6 Section E 5.7 Section E 5.9 Section E 5.10	
<i>Hold periodic lectures and educational sessions promoting environmental awareness, and practical actions that can be taken to improve the 'green' profile of the neighbourhood.</i>	Ongoing	Section E 5.10	
<i>Pursue infrastructure grant funding to implement green street improvements.</i>	Ongoing	Section E 5.13	

Implementation Action	Timing	Plan Reference	Completed
<b>Energy and Climate</b>			
<i>Review the City's Parking Bylaw to encourage additional allocation of small scale vehicle parking areas.</i>	Short Term	Section E 6.3	
<i>Amend the Development Permit Area 9 guidelines contained in Section 7.4 of the OCP to reference the Sustainable Design Guidelines.</i>	Short Term	Section E 6.1	
<i>Pursue grant funding to implement energy efficiency programs for the neighbourhood.</i>	Ongoing	Section E 6.21 Section E 6.22 Section E 6.24 Section E 6.25	
<i>Work with Vancouver Island University to explore opportunities for connecting adjacent neighbourhoods and the University to district energy systems.</i>	Medium Term	Section E 6.23	
<i>Work with the neighbourhood association and other stakeholders to establish a series of actions to meet the City's greenhouse gas emissions targets.</i>	Ongoing	Section E 6.27	
<i>Consider a review of street and laneway lighting levels to identify opportunities to implement energy efficient lighting.</i>	Medium Term	Section E 6.24 Section E 6.25	
<b>Parks, Open Space and Connectivity</b>			
<i>Develop a directional and interpretive signage program to encourage a healthy, walkable and bike friendly community.</i>	Medium Term	Section E 7.1 Section E 7.11	
<i>Work with the Island Corridor Foundation to plan and construct an extension of the E &amp; N Trail through the neighbourhood.</i>	Long Term	Section E 7.2	
<i>Consider adding parks and open space identified on Map 1 – Neighbourhood Land Use as acquisition priorities in the Parks Master Plan.</i>	Short Term	Section E 2.37	

Implementation Action	Timing	Plan Reference	Completed
<i>Develop Harewood Centennial Park as a multi-generational campus and outdoor community gathering space.</i>	Short Term	Section E 7.3 Section E 7.4	
<i>Pursue an interpretive signage program to identify natural areas, local wildlife, and heritage sites.</i>	Medium Term	Section E 7.11	
<b>Transportation and Infrastructure</b>			
<i>Update the City's Manual of Engineering Standards and Specifications for street construction in the neighbourhood to reflect street design concepts contained within this Neighbourhood Plan.</i>	Short Term	Section E 8.2	
<i>Undertake a comprehensive street plan to develop a unique identity for Harewood Village.</i>	Short Term	Section E 8.9	
<i>Review laneway conditions for road repair and opportunities for infrastructure and servicing (for example, lighting, garbage).</i>	Ongoing	Section E 8.11	
<i>Construct pedestrian greenways along the following streets:</i> <ul style="list-style-type: none"> <li>• Georgia Avenue</li> <li>• Bruce Avenue</li> <li>• Fourth Street</li> <li>• Fifth Street</li> <li>• Wakesiah Avenue</li> </ul>	Ongoing	Section E 8.15	
<i>Implement sign standards and install wayfinding signage for pedestrian and bicycle routes.</i>	Medium Term	Section E 8.17 Section E 8.21	
<i>Encourage the construction of sidewalks and curb upgrades through the City's sidewalk construction program and the Five Year Capital Plan with the following priorities:</i> <ul style="list-style-type: none"> <li>• Bruce Avenue</li> <li>• Eighth Street</li> <li>• Fifth Street</li> <li>• Fourth Street</li> <li>• Wakesiah Avenue</li> </ul>	Ongoing	Section E 8.18	

Implementation Action	Timing	Plan Reference	Completed
<p><i>Pursue traffic and pedestrian safety upgrades at the following intersections:</i></p> <ul style="list-style-type: none"> <li>• <i>Bruce Avenue at Fifth Street</i></li> <li>• <i>Fourth Street at Pine Street</i></li> <li>• <i>Third Street at Howard Avenue</i></li> </ul>	Ongoing	Section E 8.23	
<p><i>Work with the Regional District of Nanaimo Transit Department to establish transit routes through the neighbourhood, as noted on Figure 15.</i></p>	Short Term	Section E 8.26	
<p><i>Work with the Regional District of Nanaimo Transit Department and neighbourhood residents to upgrade design standards for bus stops, as laid out in Part F, Section 8.1.</i></p>	Long Term	Section E 8.27	
<p><i>Establish a program to upgrade and enhance open ditches with alternative rainwater swales.</i></p>	Medium Term	Section E 8.28	



# Glossary

## Glossary

### Affordable Housing

Means housing that does not cost more than 30% of a household's gross income. This housing may take the form of rental accommodation, home ownership, and / or other forms with support services that extend beyond financial subsidy (for example, special needs housing, assisted living facilities, transitional housing).

### Boulevard

A strip of land usually treed or vegetated, located between a sidewalk and a road or between opposing lanes of traffic in a road.

### Cluster Housing

A form of subdivision where dwelling units are grouped together, leaving the remainder of the site for open space, recreation or agriculture. Cluster housing may include a range of densities and building forms, from small lots to townhouse and other ground oriented building forms.

### Community Centre

Means a public meeting space used by members of a community for social, cultural, recreational, administrative or other purposes. This space may take the form of community space within a larger building, or as a stand alone multi purpose community building. Community Centres may or may not be publicly owned.

### Corridors

The linear focal areas for higher levels of residential densities, services, and amenities in the city on lands adjacent to urban arterials and major collector roads as designated in planNanaimo. Corridors are intended to be the focus of medium density residential and mixed use development, particularly with commercial or public space at ground level with residential in the storeys above. The Harewood Neighbourhood Plan includes three distinct Corridor Designations, the Mixed Use Corridor, Village Corridor and Residential Corridor.

### Crime Prevention Through Environmental Design (CPTED)

Designing buildings and structures, as well as the open spaces surrounding them, to reduce opportunities for crime and to increase public safety.

### Density (gross)

The number of dwelling units on a hectare of land, including land used for roads, parks, open space and all other uses. Expressed as 'units per hectare'.

### Design Guidelines

Guidelines developed during the Harewood Neighbourhood Plan process that serve as recommendations for the construction and design of residential and commercial development, along with recommendations for street design character.

### Density Bonusing

An increase in the number of dwelling units allowed on a given parcel of land in exchange for the provision of an amenity that is beneficial to the surrounding neighbourhood or community at large. Density bonusing is used as an incentive for developers to provide additional park land, open space, major community facilities, affordable housing or similar benefits. Density bonusing is typically subject to a ceiling; for example a 5% or 10% increase in the density allowed under the given zoning.

### Environmentally Sensitive Area (ESA)

Lands designated as having specific and identified environmental values requiring a higher level of protection. An ESA is an area that provides productive fish or wildlife habitat; contains sensitive, rare or depleted ecosystems and landforms; and / or represents sites of Nanaimo's natural diversity that are in danger of disappearing. ESAs provide habitat for endangered species, native or rare vegetation or wildlife, and provide wildlife corridors and secondary habitat within the city. An ESA includes lands initially defined by Environment Canada and the British Columbia Ministry of Forests, Lands and Natural Resource Operations, as sensitive ecosystems. ESAs are identified in the *Inventory of Environmentally Sensitive Areas Within the City of Nanaimo*.



## Greenhouse Gases

Gases that trap the heat of the sun in the earth's atmosphere, producing a greenhouse effect and impacting climate change.

## Greenway

A multi functional trail or strip of roadway that is used by people to recreate and commute. Intended to encourage sustainable development and a healthy lifestyle, greenways link users to the surrounding community and may include trees, gardens and other amenities such as lighting, benches, historical and interpretive signage, and bike racks.

## Ground Oriented

Dwelling units accessed from the natural grade of a lot, providing direct access to exterior space.

## Heritage

Nanaimo's resources of historically and architecturally significant buildings, structures, trees, natural landscapes and archaeological features.

## Heritage Register

A list of properties that has been recognized by Council as having heritage value.

## Infill

Development on unused or underutilized land within the existing neighbourhood.

## Infrastructure

The 'hard' services associated with development such as water, storm, and sewer services, utilities, roads and trailways.

## Land Use

Category of activity present on a property.

## Livable Neighbourhood

A community with a range of characteristics, all of which are necessary to form a complete community including:

- social, recreational and cultural opportunities that are widely available and equitably distributed throughout the city;
- opportunities for a mix of lifestyles and socio economic levels throughout the community;
- a variety of opportunities for informal public assembly;
- family, child, youth and elderly friendly urban environment;
- vibrant street life;
- affordable, accessible public transportation;
- affordable housing; and
- food security.

## Local Service Centre

Small scale commercial and community centres providing services for the day to day needs of residents within neighbourhoods.

## Mixed Use

Lands containing more than one activity; for example, a development that incorporates residential and commercial uses.

## Neighbourhood

A land use designation including, but not limited to, single family dwellings and ground oriented townhouses along major roads or in appropriate locations. A neighbourhood is a place of residence, local commercial services, and community facilities, typically defined by a unique character derived from the history, housing style, physical setting and people in that location.

## Neighbourhood Plan

A plan which encompasses the needs and desires of neighbourhoods within the city, in the context of the city wide OCP. Neighbourhood plans are intended to incorporate land use strategies that respond to the broader issues of the city in a way that contributes to creating more livable neighbourhoods.

## Official Community Plan (OCP)

A statement of goals, objectives and policies to guide decisions on planning and land use management. Adopted by City bylaw, the OCP sets out the form and character of existing and proposed land use and servicing requirements. The OCP provides a vision statement for how the community will grow and must be consistent with an adopted Regional Growth Strategy. planNanaimo is the OCP for the City of Nanaimo.

## Parks and Open Space

A land use designation that applies to lands across Nanaimo that are titled as parkland or otherwise serve as parks for recreation. These lands may be in a 'natural' state (nature parks, reserves, undevelopable lands such as flood plains and wetlands) or 'developed' state (playing fields, boulevards, squares, plazas, cemeteries). They may also be in the public domain (municipal or provincial parks, roads, pedestrian networks, commercial plazas), or in the private domain (golf courses, farms).

## Policies

Establish how objectives and goals are achieved; statement of intent or philosophy. Statements intended to guide future actions and decisions in accomplishing the Guiding Principles of the Neighbourhood Plan.

## Streetscape

The visual character of a street comprised of elements such as structures, street paving, sidewalks, public art, street furniture, landscaping, signage and lighting.

## Traffic Calming

Means of achieving a safe balance between vehicle traffic and pedestrian and cycle traffic; a process of reducing the physical and social impact of traffic, principally through mainly physical measures that reduce the negative effects of motor vehicle use, alter driver behaviour and improve conditions for motorized and non-motorized street users. Traffic calming measures may include reduced speed limits, traffic control structures, roundabouts, regulation of delivery hours, design requirements such as delivery lanes, reduced parking requirements, or increased long term parking rates.

## Urban Agriculture

The activity of growing, processing and distributing food and food related items within the city environment. Activities generally associated with urban agriculture include community gardens, backyard gardens, rooftop gardens, green streets, farmers markets and educational programs.

## View Corridor

Means an area that is essential to maintaining a scenic or other visually significant public view, and that is contained within two projected lines, from their point of intersection. View corridors are typically identified as public views where view protection applies to views from specific streets or open areas.



## Historic Points of Interest

## Historic Points of Interest

Adapted from *A Place in Time: Nanaimo Chronicles* by Jan Peterson, Nanaimo Museum, 2008

### Chase River

The river drains from Mount Benson and enters the Nanaimo watershed south of the City. It was named after an infamous chase in 1853 when sailors, HBC voyageurs and miners searched for a young Snuneymuxw man named Siam-a-sit, accused of murdering Peter Brown, a Hudson's Bay Company shepherd, near Fort Victoria. The young man hid in a hollow tree trunk in the Chase River area. It was snowing, and had he remained quiet he might have escaped, for his footprint had been obliterated, but he tried to shoot one of his pursuers. However, a flake of snow fell into the firing pan of his musket and dampened the priming charge. He was detected, captured, tried and hung along with another man at Gallows Point, off Protection Island.

*How history is recorded can often be influenced by the perspective from which it is told. What follows is the alternate version as provided by the Snuneymuxw First Nation. This story is told by Captain Moresby in a letter to his father. Courtesy of Snuneymuxw First Nation.*

In the winter of 1852-1853, two young men, a Cowichan Indian and Siam-sit, a Snuneymuxw Indian, allegedly shot Peter Brown, a shepherd working for the Hudson Bay Company at Forest Hill. In an attempt to capture Siam-sit, Governor Douglas sent out Captain Sansum Moresby of HMS Thetis with a small militia. Despite pleas from his family, the militia burnt down Siam-sit's village on the Nanaimo River and held his father hostage. In fear, Siam-sit fled to a nearby river but due to a light snow fall, the militia discovered his tracks and captured him quickly. He was given a prompt trial and hung the same day on Protection Island. This river has since been known as the Chase River.

### Colliery Dam Park

Located west of the Harewood community, from 1910 the Colliery Dams were once the reservoir for the City's water supply. Today the 28 hectare site is a popular park.

When the City began installing sewers in 1910, there were fears the city would be unable to supply the large quantity of water required for its main customer, the Western Fuel Company No. 1 Mine. The Company therefore decided to put in its own water system. There was a natural basin located one mile below the City Dam at the southwest corner of Five Acres. Engineers believed it would be ideally suited for a dam.

A spur line was laid from the Harewood Mine Railway in November 1910, allowing heavy machinery to be transported to the site.



Western Fuel Company started construction of the dams that year and completed the job on May 1, 1911. The water was carried in twelve-inch wooden stave pipes to the Coal Washers. The pipeline followed an easterly direction near Sixth Street to the Cat Stream, over a trestle, through Gordon Estate, down Robins Street to the No. 1 Mine.

Residents near the pipeline were allowed to tap into the line, and eventually this water was carried to most of the homes in South Harewood. The dams created an upper and lower reservoir, with a spillway at the eastern end to siphon off overflow. The Colliery Dam Park was later incorporated into the City of Nanaimo in 1975, when the Harewood area was amalgamated with the City.

### Robins Park

This small 7.15 acre park in South Nanaimo was named for Samuel Robins, the Vancouver Coal Company's superintendent. The site was originally constructed in 1903-1904 as a cricket field. It was officially dedicated as a park in 1980.

The Cricket Field was described as having "magnificent turf". It was created when businessman John Hilbert and G. Crutchley met with Robins on June 5, 1889, and negotiated permission for the Cricket Club to use a cleared area for their matches. The land lay in the Daisy Field area, between the E & N Railway tracks and Chinatown at Needham Street. Robert Hilton cared for the field. The Cricket club held its first important match in 1892. Horse racing was also held in the oval track on the grounds. Empire Day celebrations were held here, a community event that usually climaxed with an afternoon of horse racing. Future plans for the park included a half-mile track, four tennis courts, a bowling green and a five thousand seat grandstand. The plans were shelved in favour of developing Bowen Park however.

### Third Chinatown

Harewood was also the historical location, in part, of Nanaimo's Third Chinatown. The first chinatown was a scattered collection of buildings in the Victoria Crescent / Esplanade Street area. The second Chinatown consisted of eight acres outside the City limits in the View Street / Princess Street area, set aside for the Chinese by the coal company in 1884. Here, the Chinese were permitted to build dwellings and shops rent-free, except that workers on the payroll were levied a tax of one dollar per month. Around 1908, a Chinese merchant acquired the lease on the property and introduced rents. In response, resentful residents formed a mutual society, purchased the property at Hecate Street and Pine Street and moved all the buildings to the "Third Chinatown" location. Roughly half the buildings were located in what would later become the Harewood Improvement District. In 1960, most of the buildings were destroyed in a devastating fire.



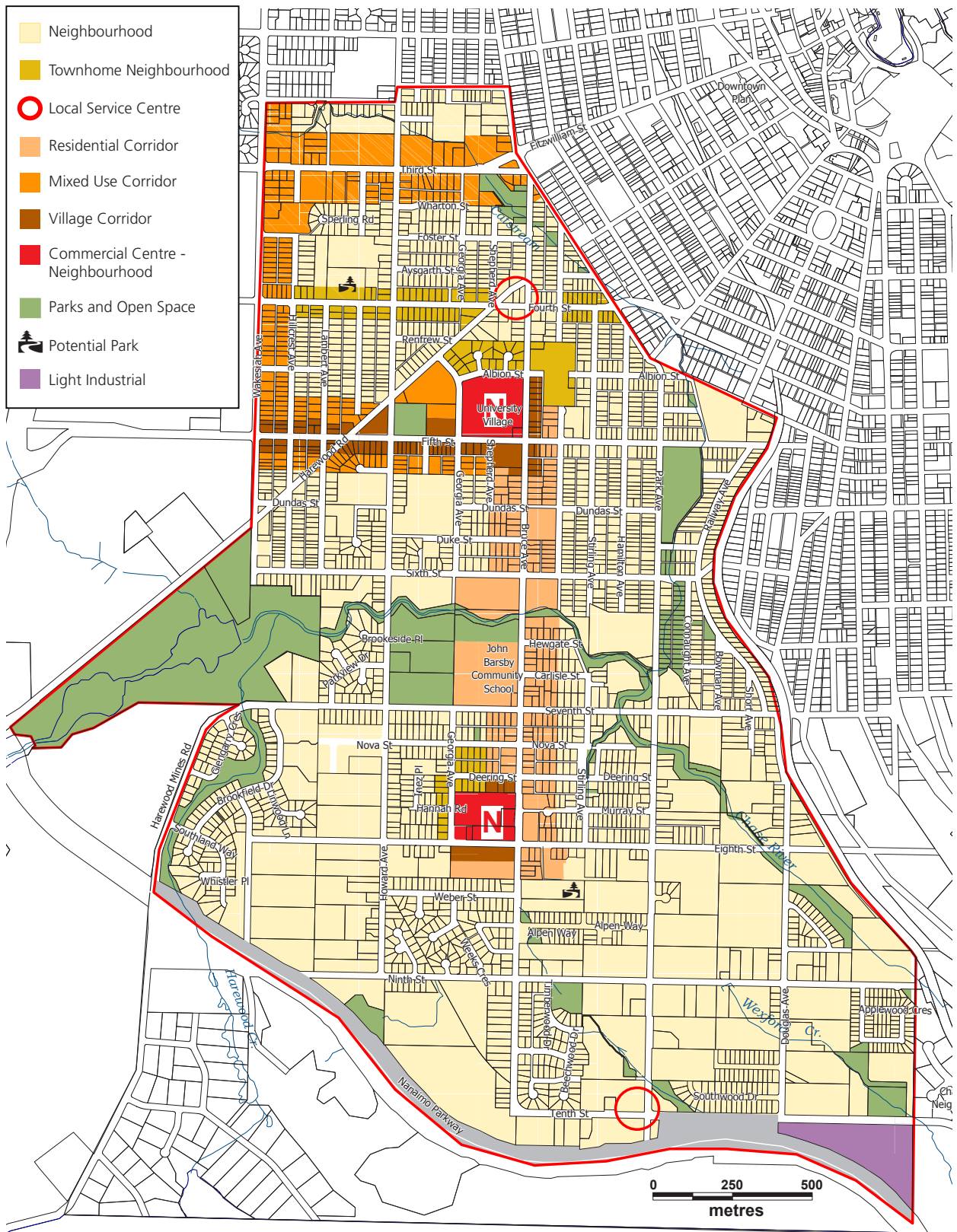
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# Mapping

**Map 1** Neighbourhood Land Use

**Map 2** Pedestrian and Road Network

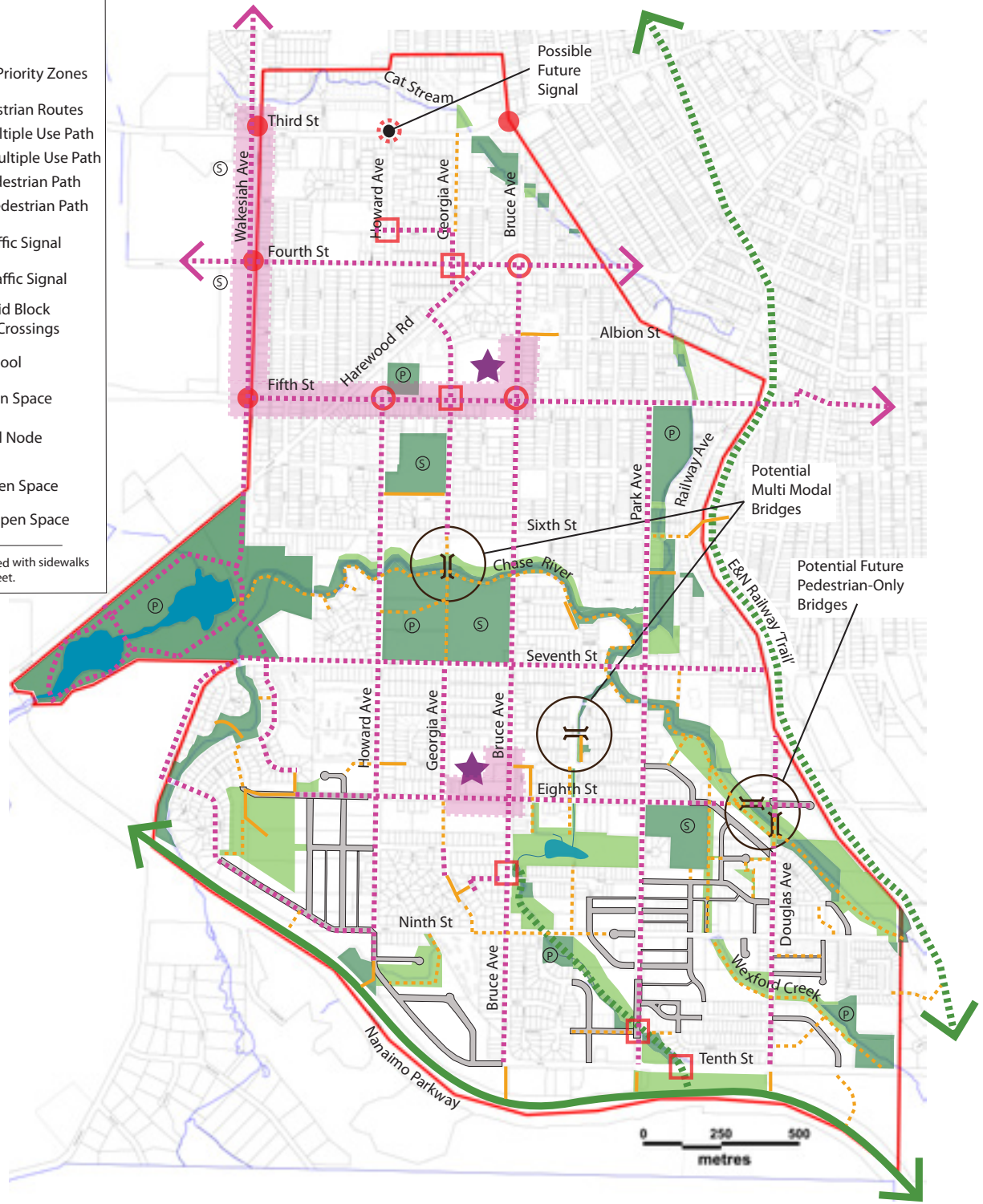


Map 1 - Neighbourhood Land Use



### Pedestrian Network Plan

- Pedestrian Priority Zones
  - Major Pedestrian Routes
  - Existing Multiple Use Path
  - Potential Multiple Use Path
  - Existing Pedestrian Path
  - Potential Pedestrian Path
  - Existing Traffic Signal
  - Potential Traffic Signal
  - Potential Mid Block Pedestrian Crossings
  - S Existing School
  - P Parks + Open Space
  - Commercial Node
  - Existing Open Space
  - Proposed Open Space
- All roads to be developed with sidewalks on both sides of the street.



Map 2 - Pedestrian and Road Network

